

Development Applications

Notice is hereby given under Section 57(3) of the *Land Use Planning & Approvals Act 1993* that an application has been made to the Break O' Day Council for a permit for the use or development of land as follows:

DA Number	DA 2026 / 00035
Applicant	Thomas Wolf Developments
Proposal	Residential - Construction of Five (5) Dwellings (Multi-Residential Development) and Demolition of Outbuildings
Location	32 Medeas Cove Esplanade, St Helens (CT26880/1)

Plans and documents can be inspected at the Council Office by appointment, 32 – 34 Georges Bay Esplanade, St Helens during normal office hours or online at www.bodc.tas.gov.au.

Representations must be submitted in writing to the General Manager, Break O' Day Council, 32 -34 Georges Bay Esplanade, St Helens 7216 or emailed to admin@bodc.tas.gov.au, and referenced with the Application Number in accordance with section 57(5) of the abovementioned Act during the fourteen (14) day advertised period commencing on Saturday 4th July, 2026 **until 5pm Friday 17th July, 2026.**

John Brown
GENERAL MANAGER

SITE INFORMATION

Title Reference Number: **26880/1**
Wind Classification: **TBC**
Soil Classification: **TBC**
Climate Zone: **7**
BAL Level: **N/A**
Alpine Area: **TBC**
Corrosion Environment: **Moderate**

Volume / Folio
Refer to Site Assessment Report
Refer to Site Assessment Report
TBC
NA / Refer to Site Assessment Report
Refer to NCC
For steel subject to the influence of salt water, breaking surf or heavy industrial areas, Medium; NCC Vol. 2 Part 7.2.2 (sheet roofing) & Part 6.3.9 (Structural Steel). Cladding and fixings to manufacturer's recommendations.

Other Hazards: **TBC**

High wind, earthquake, flooding, landslip, dispersive soils, sand dunes, mine subsistence, landfill, snow & ice or other relevant factors.

NCC/BCA 2022 Note:

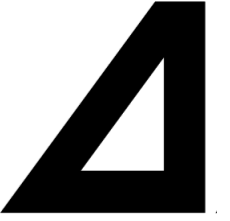
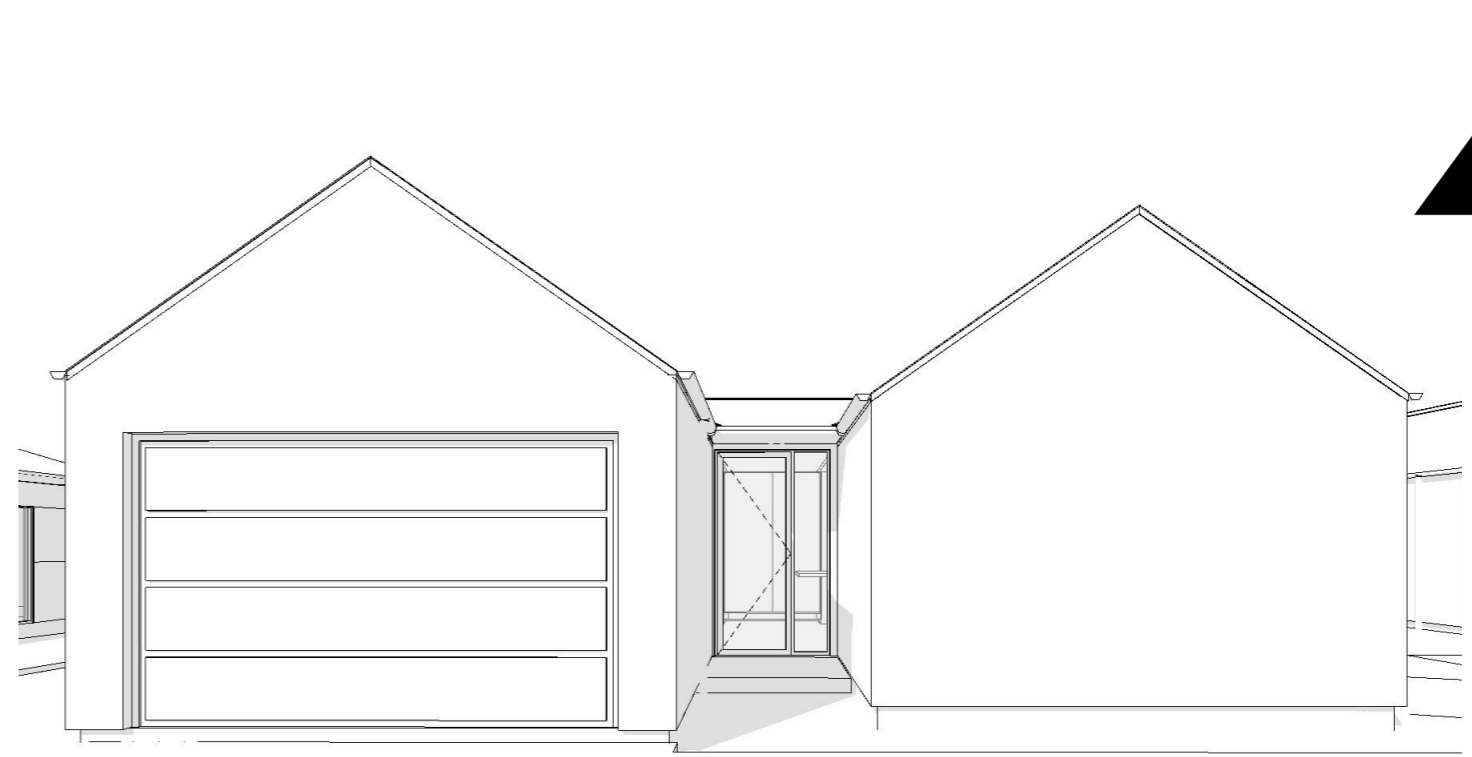
All NCC references are to National Construction Code NCC 2022 Volume 2 & Housing Provisions.

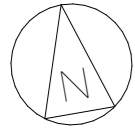
AREAS

Land Area- Total 2,245m²
Building Area- Total 800m²
Site Coverage 35.63%

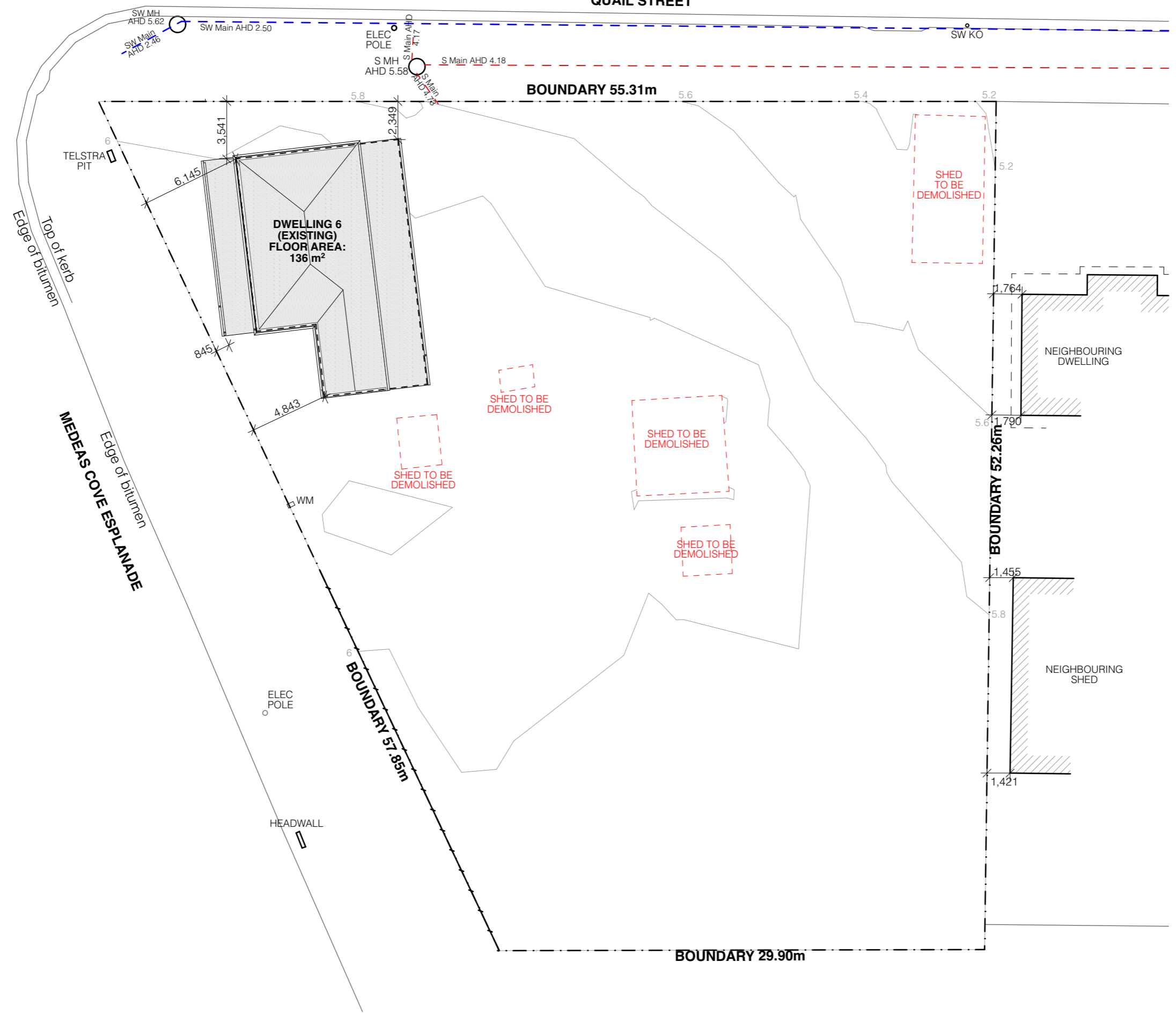
DRAWING SCHEDULE

ID	Layout Name
01	Cover
02	Site Plan- Existing
03	Site Plan- Proposed
04	Site Plan- Planning overlays
05	Site Plan- Setbacks
06	Site Plan- Plumbing
07	Site Plan- Traffic
08	Elevations- Site
09	Floor Plan- Proposed Dwelling 1
10	Elevation- Proposed Dwelling 1
11	Floor Plan- Proposed Dwelling 2
12	Elevation- Proposed Dwelling 2
13	Floor Plan- Proposed Dwelling 3
14	Elevation- Proposed Dwelling 3
15	Floor Plan- Proposed Dwelling 4
16	Elevation- Proposed Dwelling 4
17	Floor Plan- Proposed Dwelling 5
18	Elevation- Proposed Dwelling 5
19	Floor Plan- Existing Dwelling (6)
20	Elevation- Existing Dwelling (6)
21	Shadow Diagrams- June 21- 9am
22	Shadow Diagrams- June 21- 10am
23	Shadow Diagrams- June 21- 11am
24	Shadow Diagrams- June 21- 12pm
25	Shadow Diagrams- June 21- 1pm
26	Shadow Diagrams- June 21- 2pm
27	Shadow Diagrams- June 21- 3pm
28	Planning Response- 8.4.1
29	Planning Response- 8.4.2, 8.4.3, 8.4.4, 8.4.6
30	Planning Response- 8.4.8, C7.6.1, BRE-S2.7.1



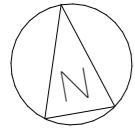


Site Plan- Existing
scale 1:250



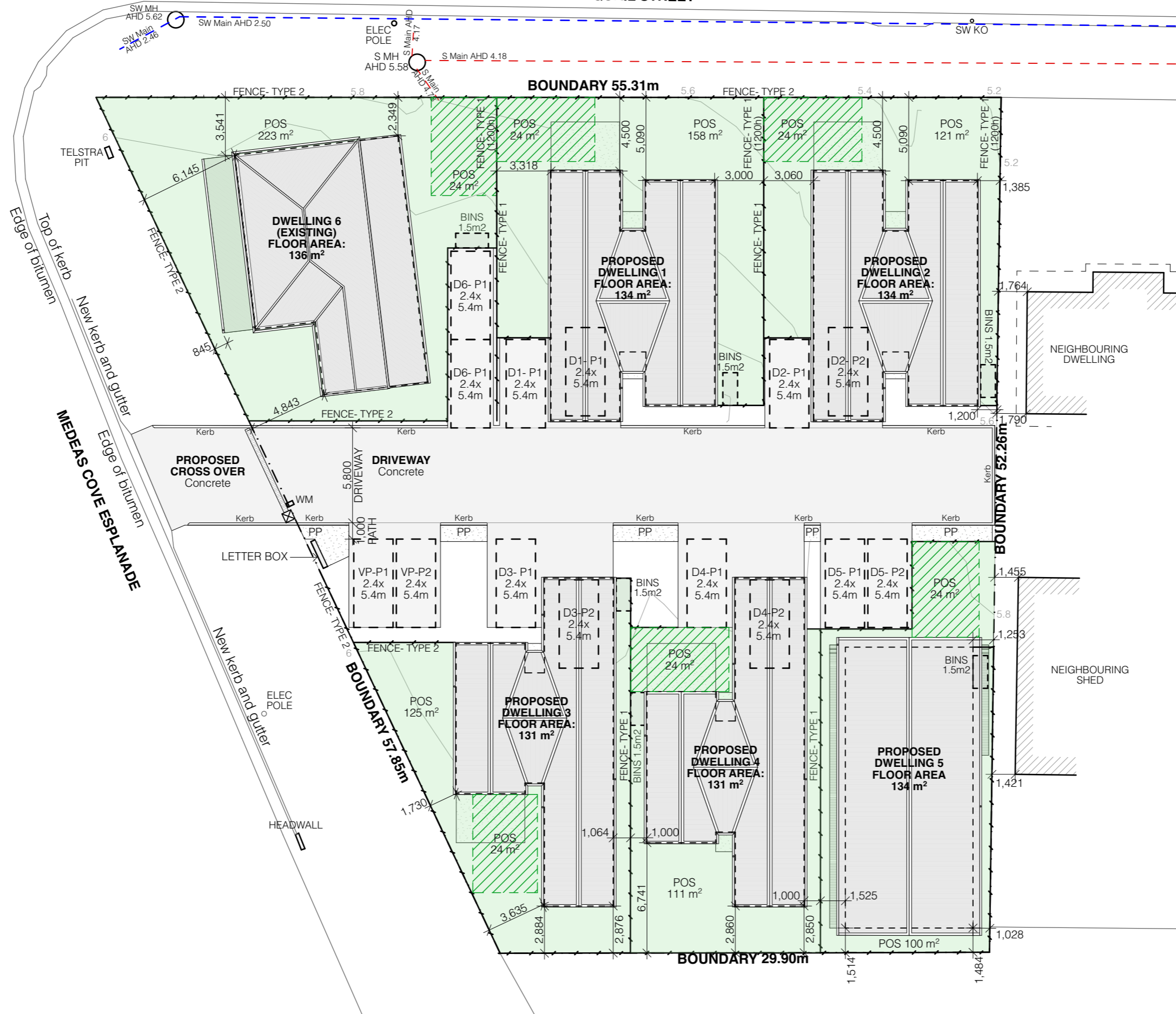
LEGEND

Stormwater 100mm UPVC	---
Ag drain 100mm socked slotted	-.-.-
Sewer 100mm UPVC	---
Water	---
MH	Man Hole
SW	Stormwater
SW KO	Stormwater Kerb Outlet (Existing)
S	Sewer
WM	Water Meter (Existing)



Site Plan- Proposed
scale 1:250

QUAIL STREET



LEGEND

- Stormwater 100mm UPVC ---
- Ag drain 100mm socked slotted ---
- Sewer 100mm UPVC ---
- Water ---
- PP Pedestrian Path
- MH Man Hole
- SW Stormwater
- SW KO Stormwater Kerb Outlet (Existing)
- S Sewer
- WM Water Meter (Existing)

Fence- Type 1 Paling fence 1800h. Fence to taper to 1200h within 4.5m of front boundary.

Fence- Type 2 Timber picket fence 1200h

GENERAL NOTES

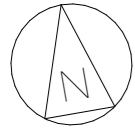
Any services must be located onsite prior to excavation

Exact locations of existing buildings and boundaries are subject to survey and must be confirmed onsite prior to construction

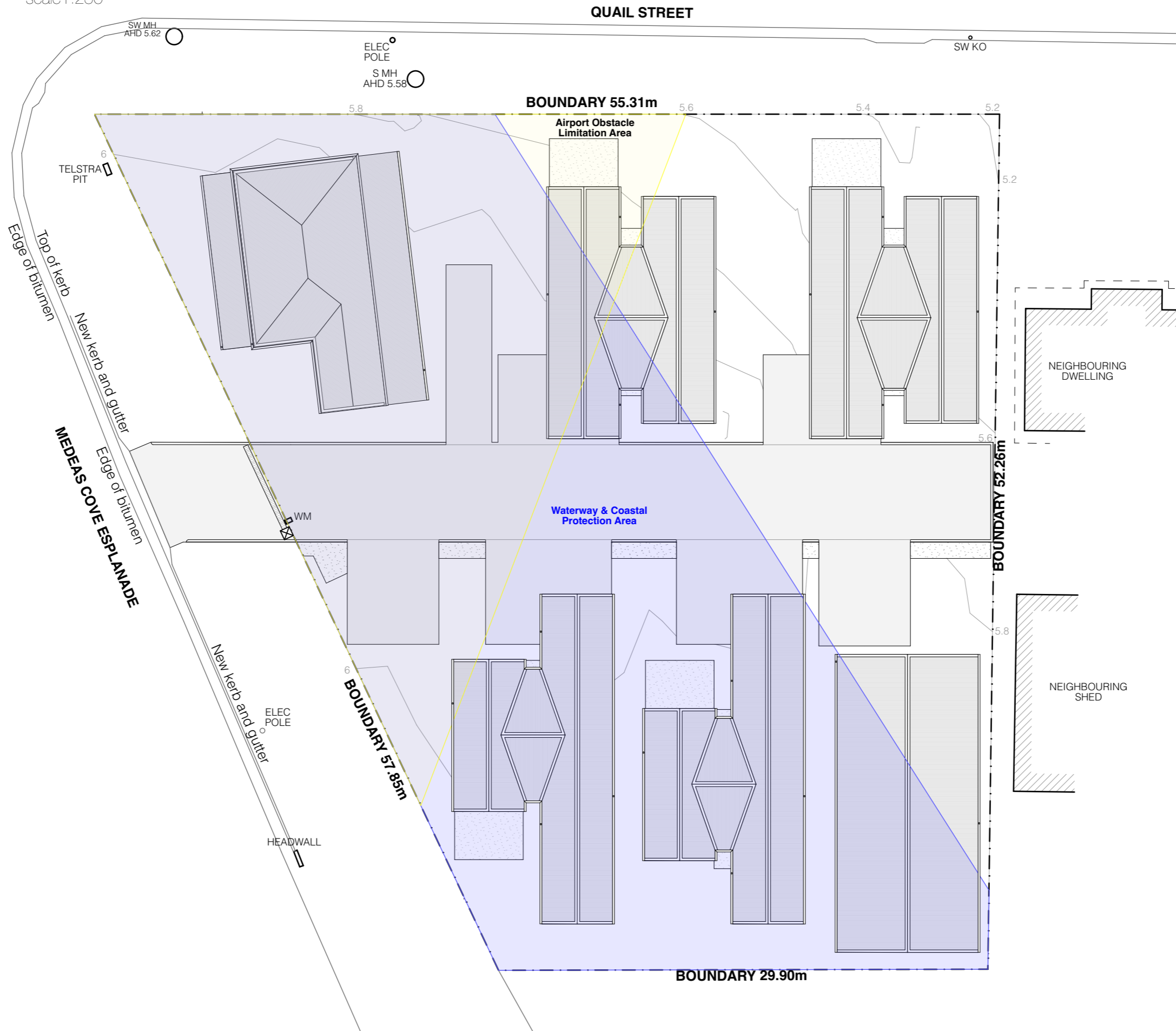
Bulk excavation must not impede on nearby structures, neighbouring property nor boundary fences

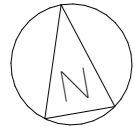
Surface drainage shall be designed and constructed to avoid water ponding against or near the footing in accordance with AS2870. The ground in the immediate vicinity of the perimeter footing, including the ground uphill from the slab on cut-and-fill sites, shall be graded to fall 50 mm minimum away from the footing over a distance of 1m and shaped to prevent ponding of water. Where filling is placed adjacent to the building, the filling shall be compacted and graded to ensure drainage of water away from the building.

All works must be in accordance with relevant Australian Standards, the National Construction Code, manufacturers specification and all other relevant regulatory bodies including local council

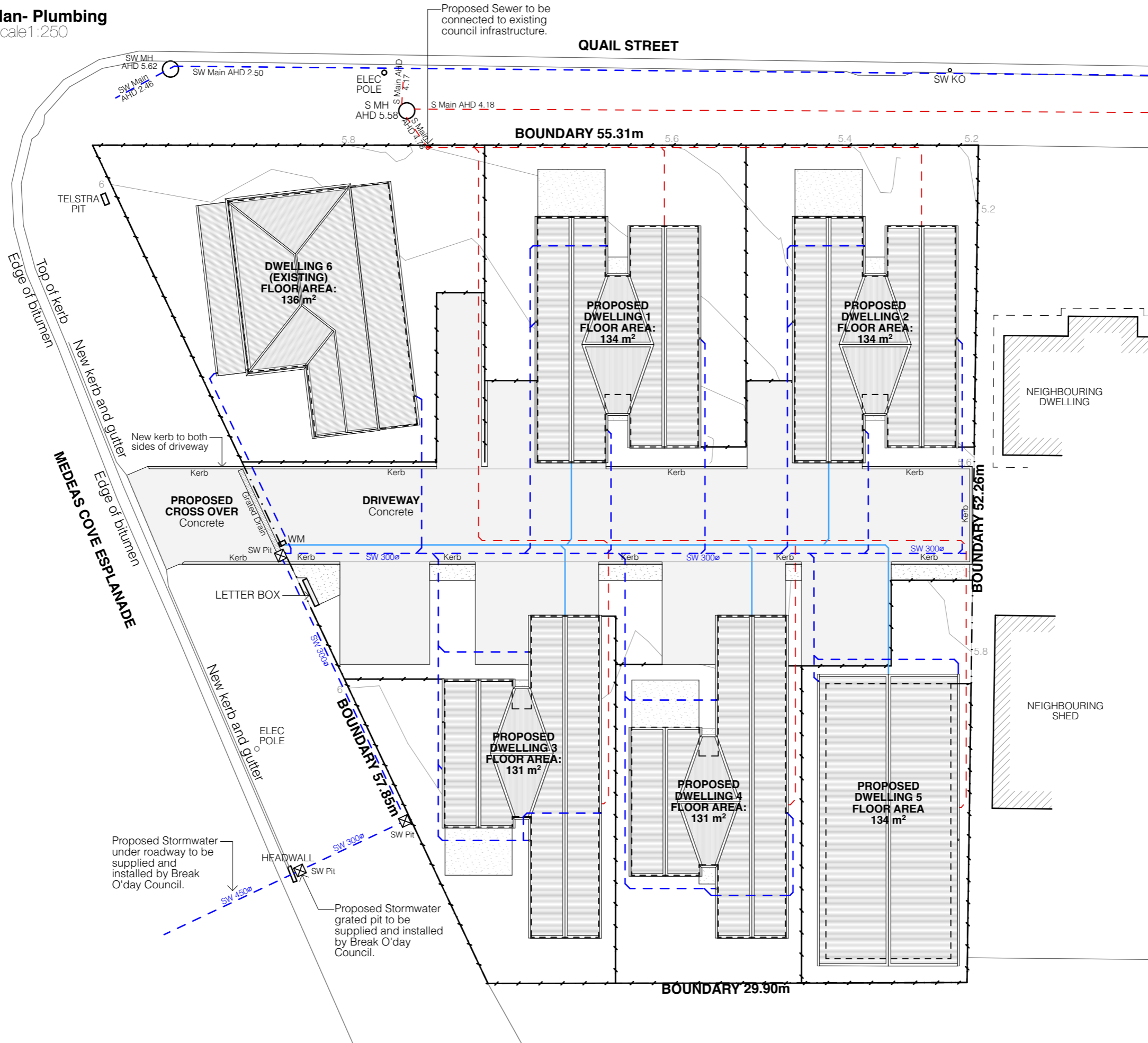


Site Plan- Planning overlays
scale 1:250



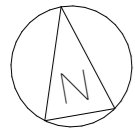


Site Plan- Plumbing
scale 1:250



LEGEND

Stormwater 100mm UPVC	---
Ag drain 100mm socked slotted	---
Sewer 100mm UPVC	---
Water	---
MH	Man Hole
SW	Stormwater
SW KO	Stormwater Kerb Outlet (Existing)
S	Sewer
WM	Water Meter (Existing)



Site Plan- Traffic
scale 1:250, 1:100

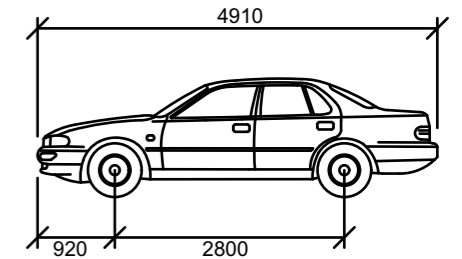
QUAIL STREET

LEGEND

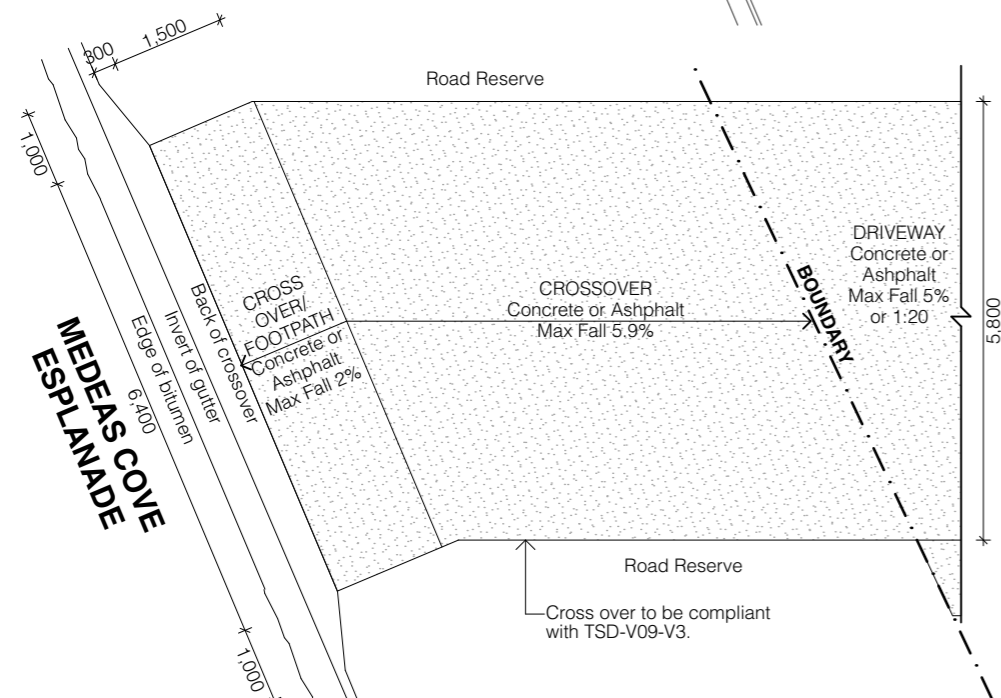
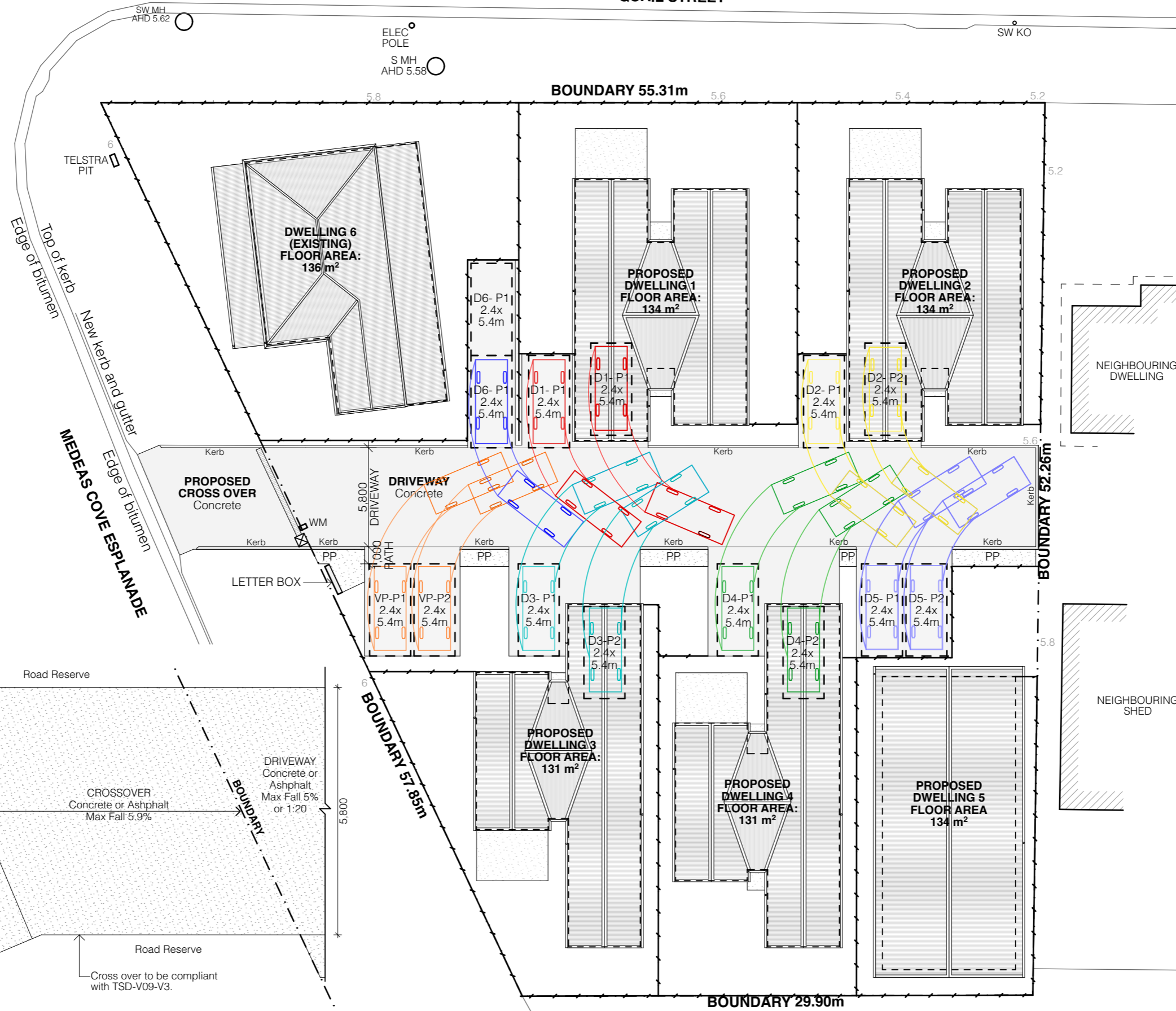
- PP Pedestrian Path
- VP Visitor Car Space

GENERAL NOTES

- Vehicle Movement:**
1. Movement templates demonstrate the ability of vehicles to enter in a forward direction and reverse.
 2. The base dimension of the car template represents the B85 (85th percentile) vehicle.
 3. The swept path of the vehicle represent the outer extents of the vehicle body.
 4. The paths were generated with a turning speed of 5km/h.



B85 vehicle	mm
Width	1870
Track	1770
Lock to Lock Time	6000
Steering Angle	3400



Plan- Crossover
scale 1:100

32 Medeas Cove Esplanade, St Helens TAS 7216
Thomas Wolf Developments

NOT FOR CONSTRUCTION

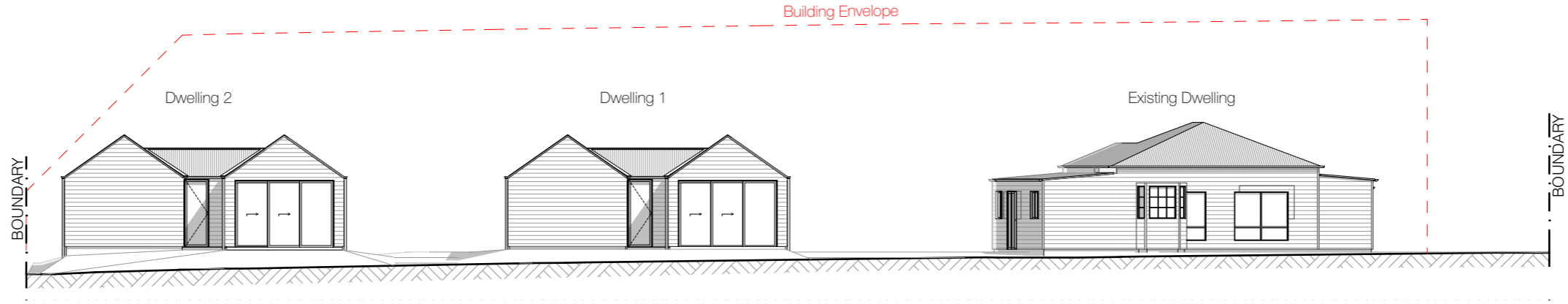


www.axishomes.com
ABN 31 639 378 751
Ph 0476 662 062
info@axishomes.com

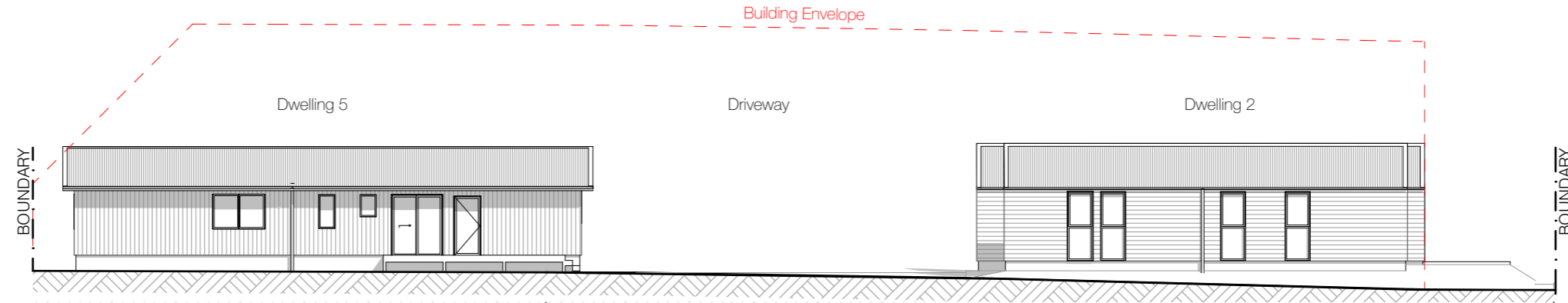
Date Published **22/6/2026**
Drawing Status **DA**
Drawing Scale **1:250, 1:100**
Drawn by **NT**

Revision
PAGE
07 of 30

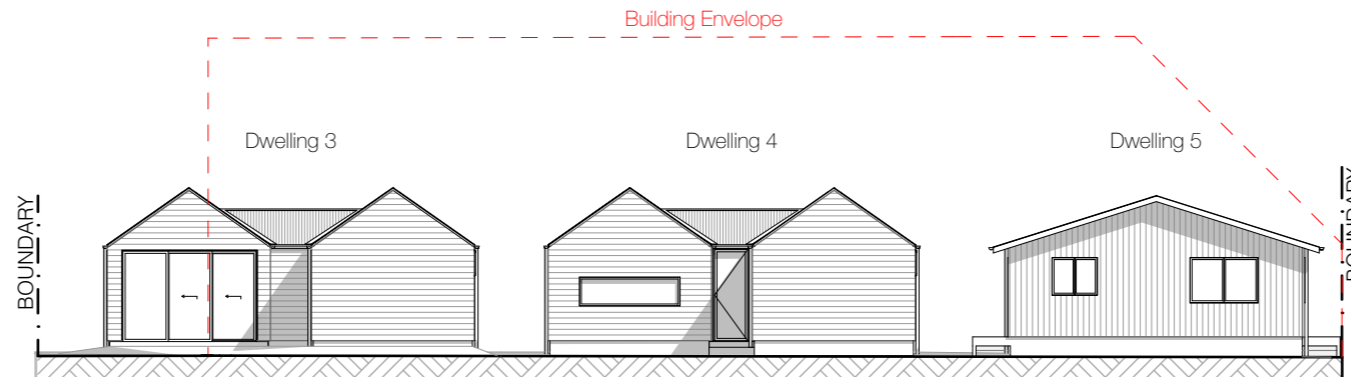
Elevations- Site
scale 1:200



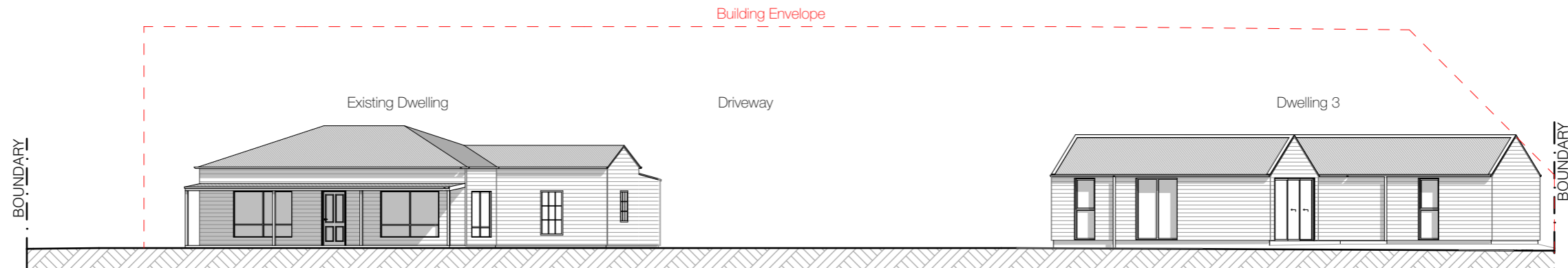
North Elevation
scale 1:200



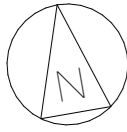
East Elevation
scale 1:200



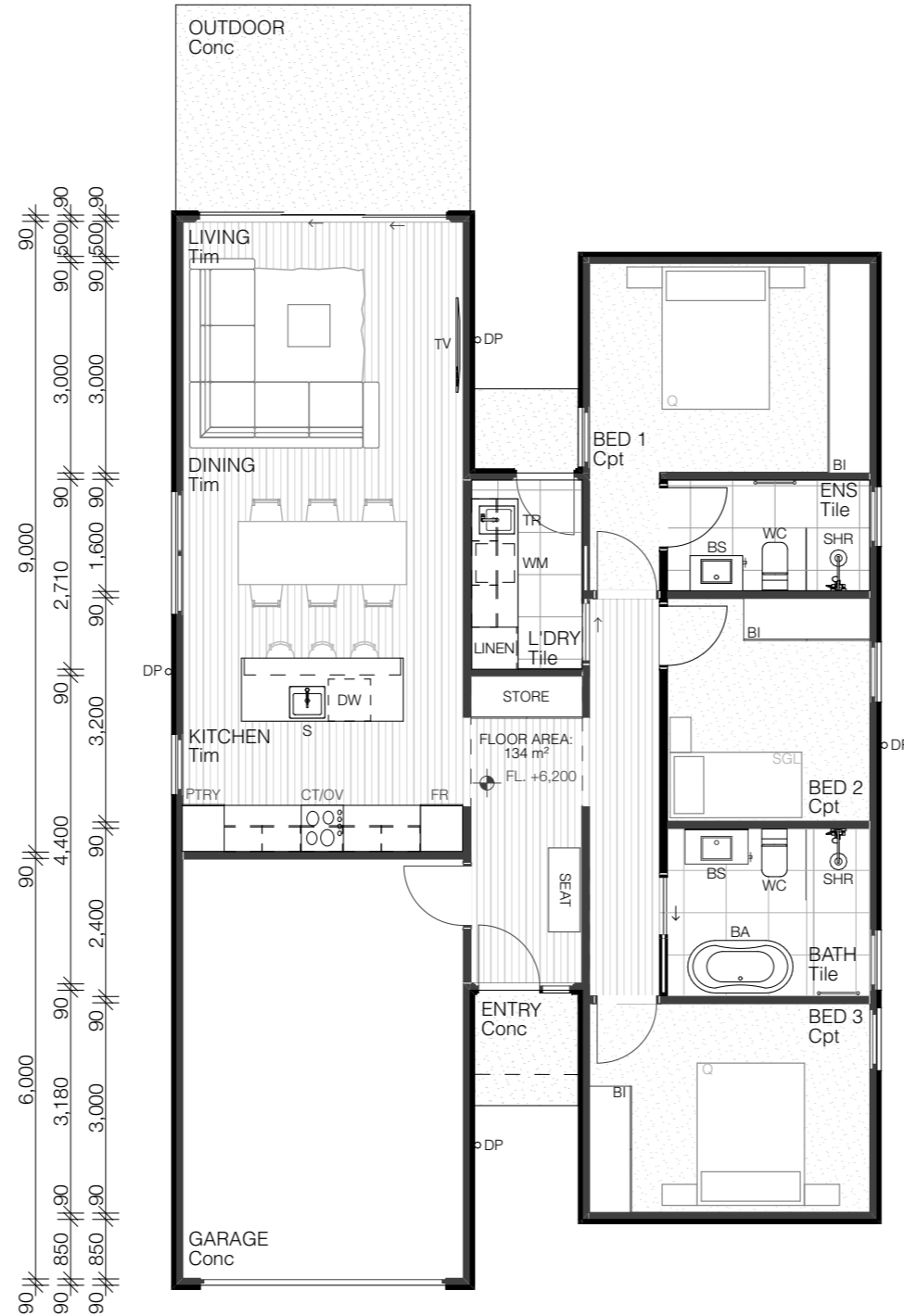
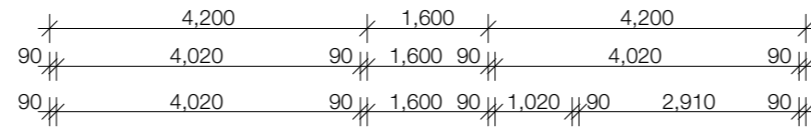
South Elevation
scale 1:200



West Elevation
scale 1:200



Floor Plan- Proposed Dwelling 1
scale 1:100



LEGEND

- █ Proposed wall
- ⊕ Existing levels
- ⊕ New levels
- FL - Finished floor level (top of particle board / slab)

Fixtures & Fittings

- AC Air Conditioner
- AC-OUT Air Conditioner- Outdoor Unit
- BI Built-in Cupboard
- BS Basin
- CJ Control joint
- CT Cooktop
- COL Column
- CT Cooktop
- CL Ceiling level
- DP Down pipe
- FFL Finished Floor level
- FR Fridge
- FP Fire Pit
- GB Glass balustrade
- GD Garage Door
- GRD Grated Drain
- HWS Hot Water System
- LDRY Laundry
- MB Meter box
- NBN NBN Modem
- NBN-EXT NBN connection box
- OV Oven
- RWH Rainwater Head
- SHR Shower
- SK Skylight
- S Sink
- TAP Water Tap
- TR Trough
- TV Television
- WC Toilet
- WH Wood Heater
- WM Washing Machine
- WIR Walk-in Robe
- WIP Walk-in Pantry

Material/ Finish

- Conc Concrete paving slab.
- Cpt Carpet
- Paving Paving
- Scrn Screen
- Tile Floor tile
- Tim Plank flooring

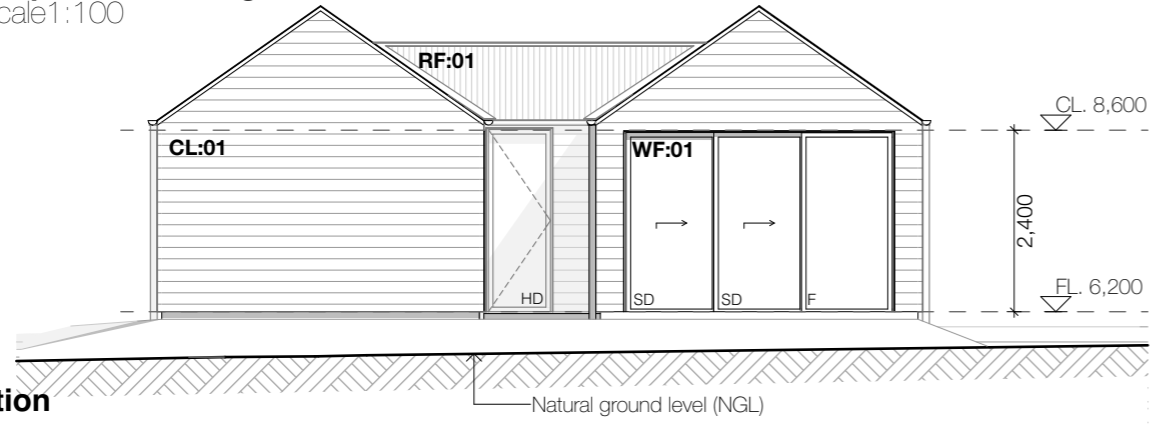
GENERAL NOTES

Finished landscaping must prevent water build up near footings, onto nearby structures and the driveway by providing sufficient fall and drainage systems in accordance with Australian Standards

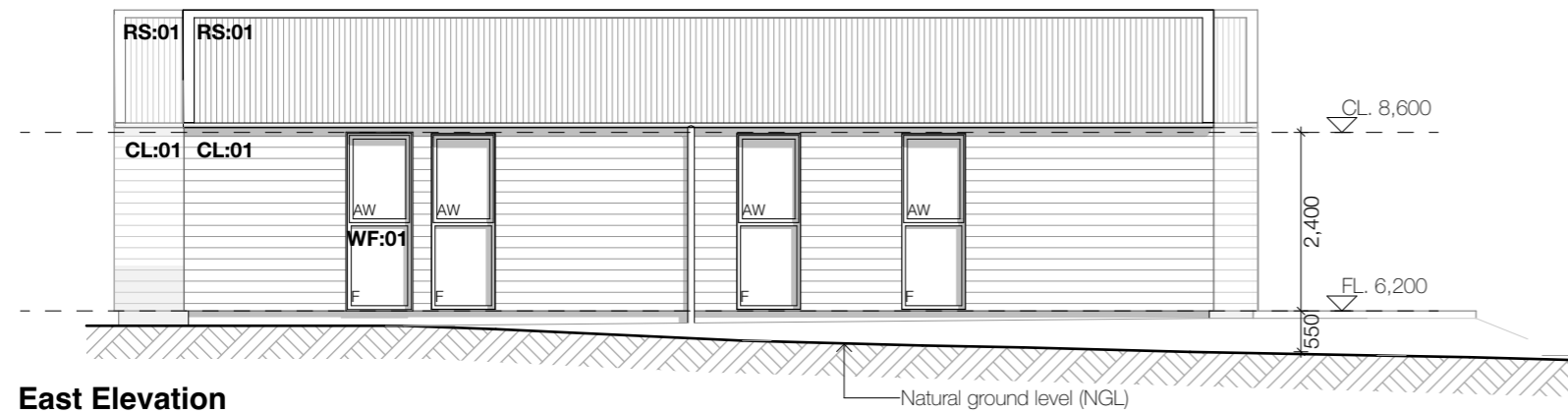
All works must be in accordance with relevant Australian Standards, the National Building Code, manufacturers specification and all other relevant regulatory bodies including local council.

Refer to Builder's scope of works for joinery inclusions.

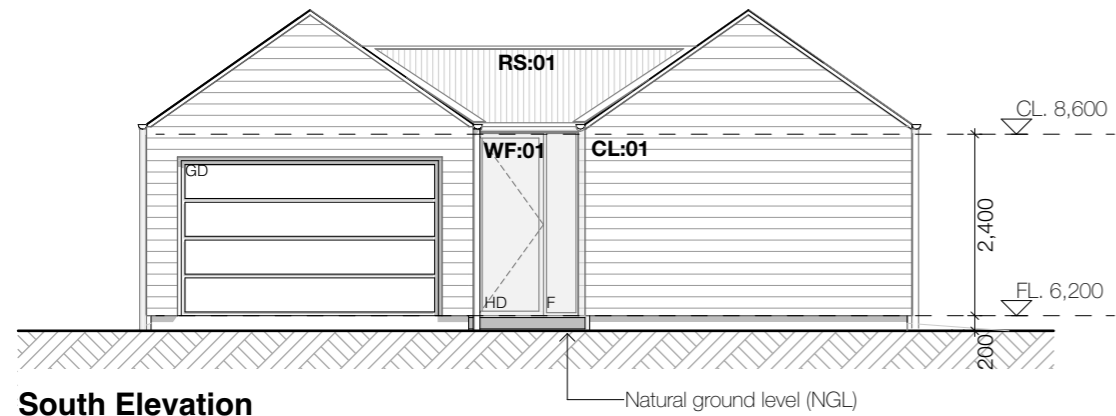
Elevation- Proposed Dwelling 1
scale 1:100



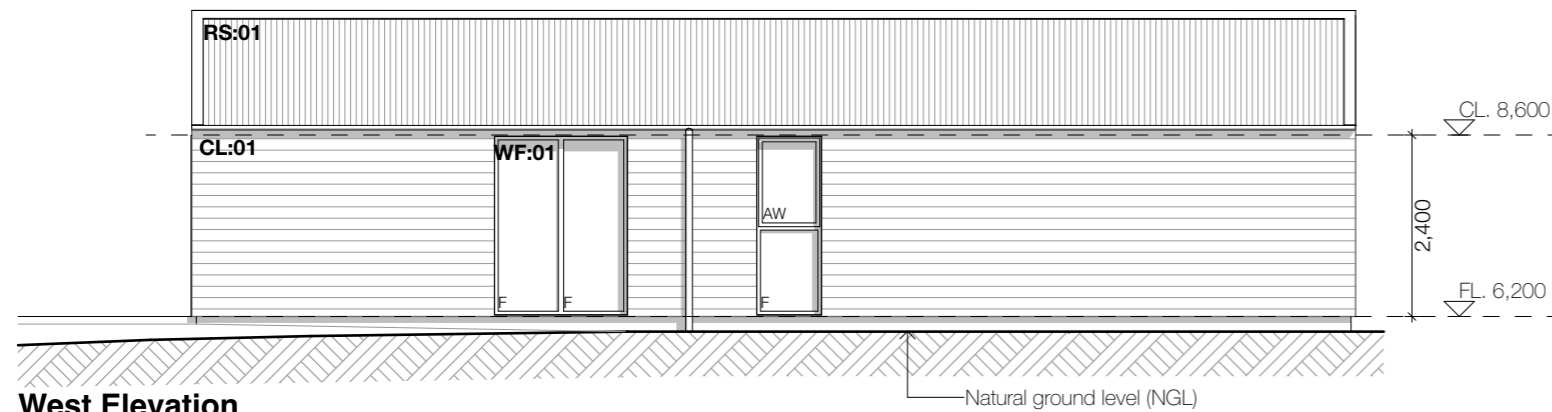
North Elevation
scale 1:100



East Elevation
scale 1:100



South Elevation
scale 1:100



West Elevation
scale 1:100

LEGEND

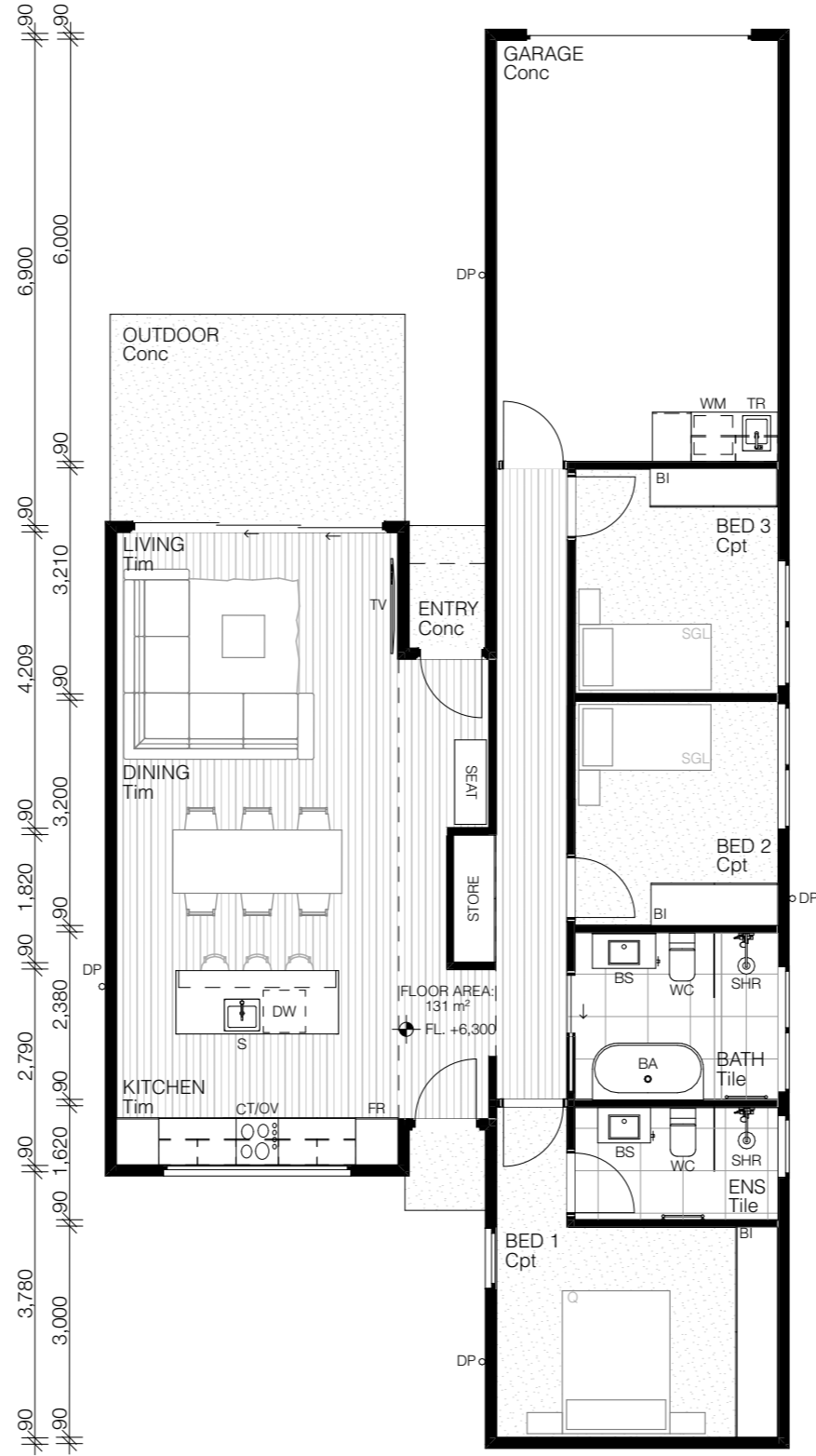
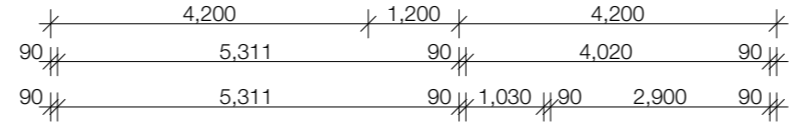
- AW - Awning window
- BB - Batten balustrade
- CJ - Control joint
- CL - Ceiling level
- DP - Downpipe
- F - Fixed window
- GB - Glass balustrade
- GD - Garage door
- HD - Glass door
- FL - Finished Floor level
- SD - Sliding door
- SW - Sliding window
- TAP - Water tap
- WB - Wire balustrade

SCHEDULE

- CL:01 Fibre Cement
Type: JH Linea
Colour: Surfmist
Finish: Paint
- RS:01 Roofing
Deck: Corrugated metal 0.42bmt
Colour: Surfmist
- WF:01 Window and External Doors
Type: Double glazed aluminium frame
Glass Tint: TBC
Frame Colour: White



Floor Plan- Proposed Dwelling 2
scale 1:100



FLOOR AREA: 131 m²
FL. +6,300

LEGEND

- █ Proposed wall
- ⊕ Existing levels
- ⊕ New levels
- FL - Finished floor level (top of particle board / slab)

Fixtures & Fittings

- AC Air Conditioner
- AC-OUT Air Conditioner- Outdoor Unit
- BI Built-in Cupboard
- BS Basin
- CJ Control joint
- CT Cooktop
- COL Column
- CT Cooktop
- CL Ceiling level
- DP Down pipe
- FFL Finished Floor level
- FR Fridge
- FP Fire Pit
- GB Glass balustrade
- GD Garage Door
- GRD Grated Drain
- HWS Hot Water System
- LDRY Laundry
- MB Meter box
- NBN NBN Modem
- NBN-EXT NBN connection box
- OV Oven
- RWH Rainwater Head
- SHR Shower
- SK Skylight
- S Sink
- TAP Water Tap
- TR Trough
- TV Television
- WC Toilet
- WH Wood Heater
- WM Washing Machine
- WIR Walk-in Robe
- WIP Walk-in Pantry

Material/ Finish

- Conc Concrete paving slab.
- Cpt Carpet
- Paving Paving
- Scrn Screen
- Tile Floor tile
- Tim Plank flooring

GENERAL NOTES

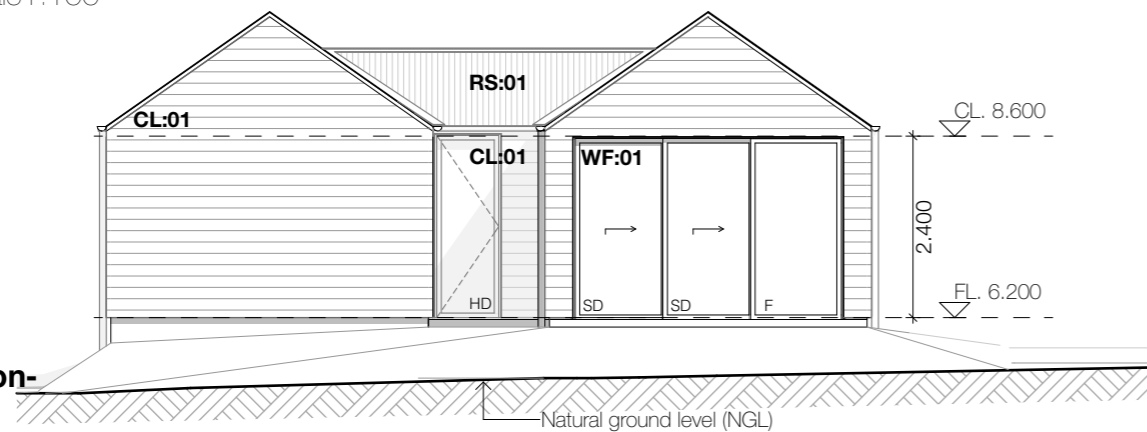
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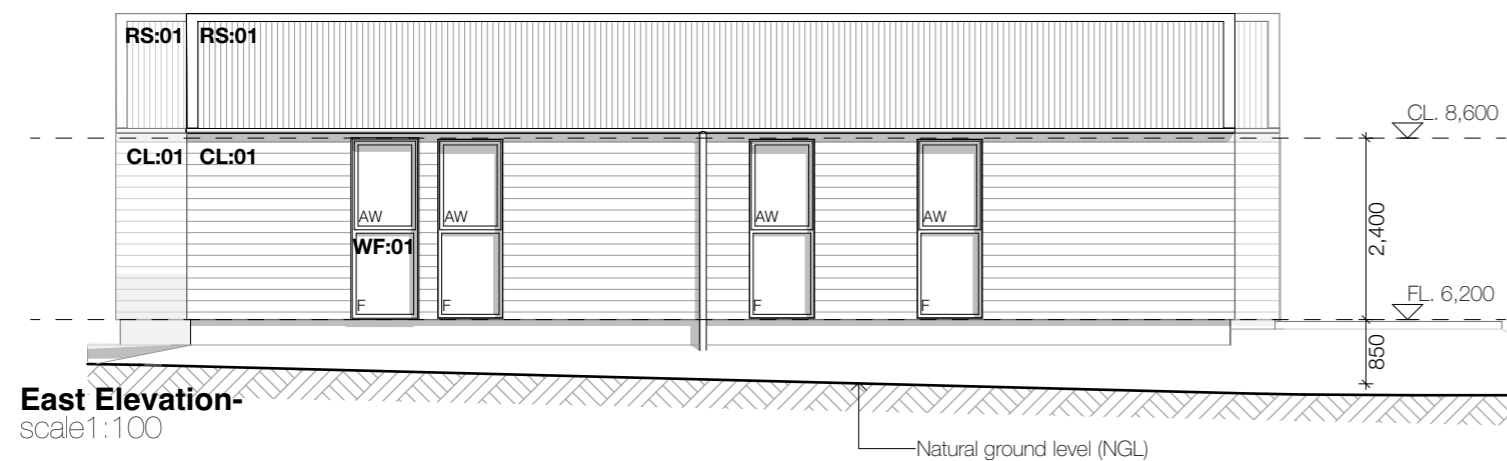
Refer to Builder's scope of works for joinery inclusions.

Elevation- Proposed Dwelling 2

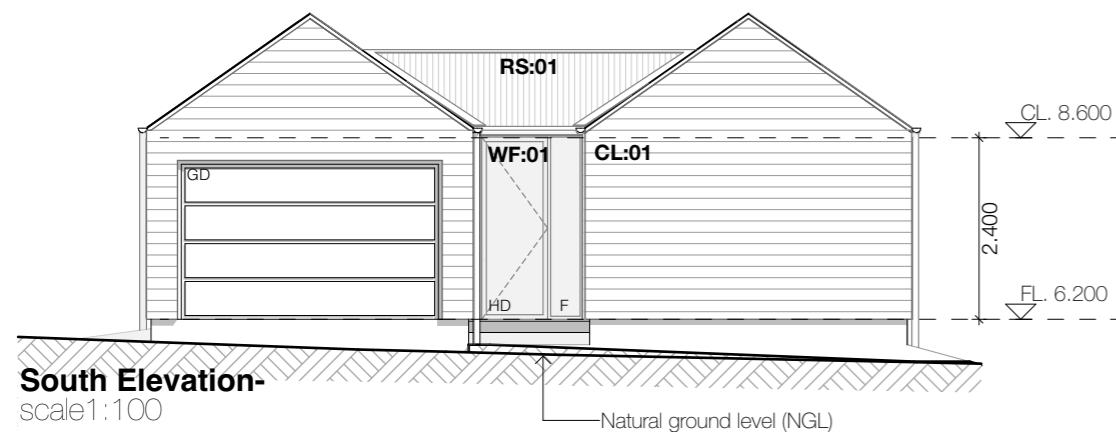
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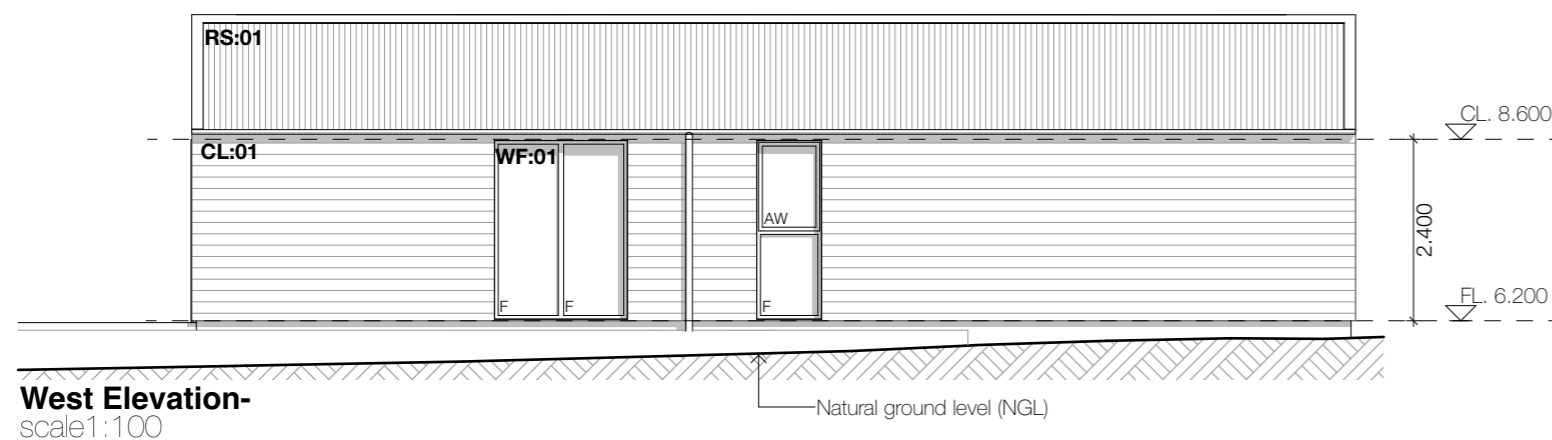
North Elevation-
scale 1:100



East Elevation-
scale 1:100



South Elevation-
scale 1:100



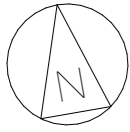
West Elevation-
scale 1:100

LEGEND

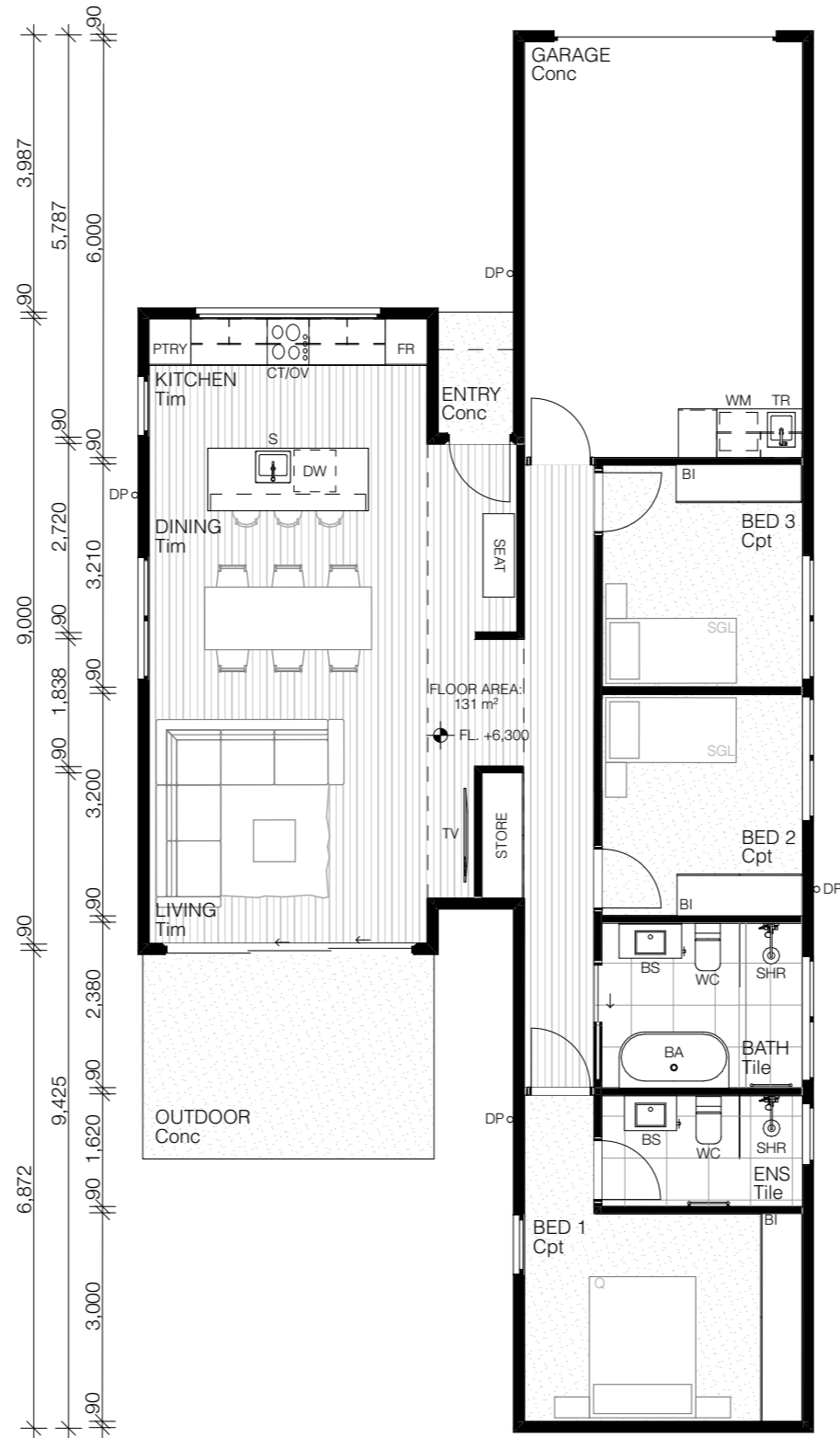
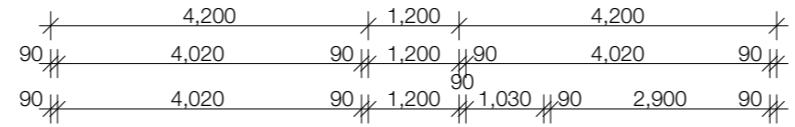
- AW - Awning window
- BB - Batten balustrade
- CJ - Control joint
- CL - Ceiling level
- DP - Downpipe
- F - Fixed window
- GB - Glass balustrade
- GD - Garage door
- HD - Glass door
- FL - Finished Floor level
- SD - Sliding door
- SW - Sliding window
- TAP - Water tap
- WB - Wire balustrade

SCHEDULE

- CL:01 Fibre Cement
Type: JH Linea
Colour: Surfmist
Finish: Paint
- RS:01 Roofing
Deck: Corrugated metal 0.42bmt
Colour: Surfmist
- WF:01 Window and External Doors
Type: Double glazed aluminium frame
Glass Tint: TBC
Frame Colour: White



Floor Plan- Proposed Dwelling 3
scale 1:100



LEGEND

- █ Proposed wall
- ⊕ Existing levels
- ⊕ New levels
- FL - Finished floor level (top of particle board / slab)

Fixtures & Fittings

- AC Air Conditioner
- AC-OUT Air Conditioner- Outdoor Unit
- BI Built-in Cupboard
- BS Basin
- CJ Control joint
- CT Cooktop
- COL Column
- CT Cooktop
- CL Ceiling level
- DP Down pipe
- FLL Finished Floor level
- FR Fridge
- FP Fire Pit
- GB Glass balustrade
- GD Garage Door
- GRD Grated Drain
- HWS Hot Water System
- LDRY Laundry
- MB Meter box
- NBN NBN Modem
- NBN-EXT NBN connection box
- OV Oven
- RWH Rainwater Head
- SHR Shower
- SK Skylight
- S Sink
- TAP Water Tap
- TR Trough
- TV Television
- WC Toilet
- WH Wood Heater
- WM Washing Machine
- WIR Walk-in Robe
- WIP Walk-in Pantry

Material/ Finish

- Conc Concrete paving slab.
- Cpt Carpet
- Paving Paving
- Scrn Screen
- Tile Floor tile
- Tim Plank flooring

GENERAL NOTES

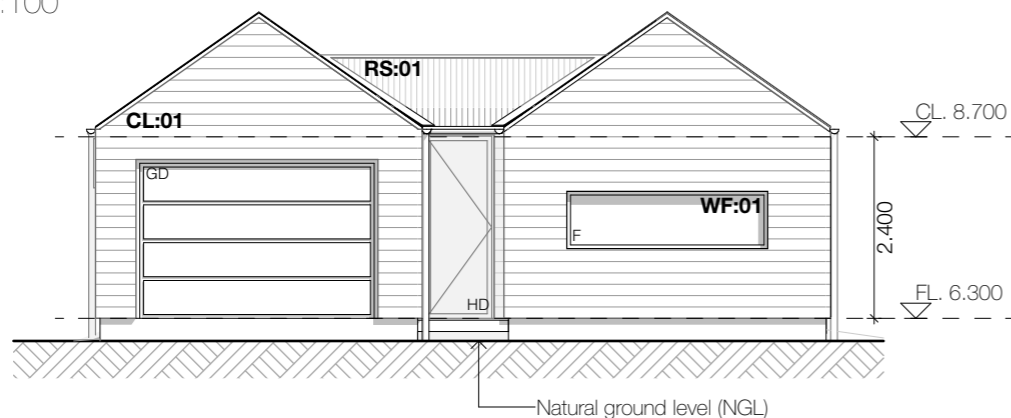
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Refer to Builder's scope of works for joinery inclusions.

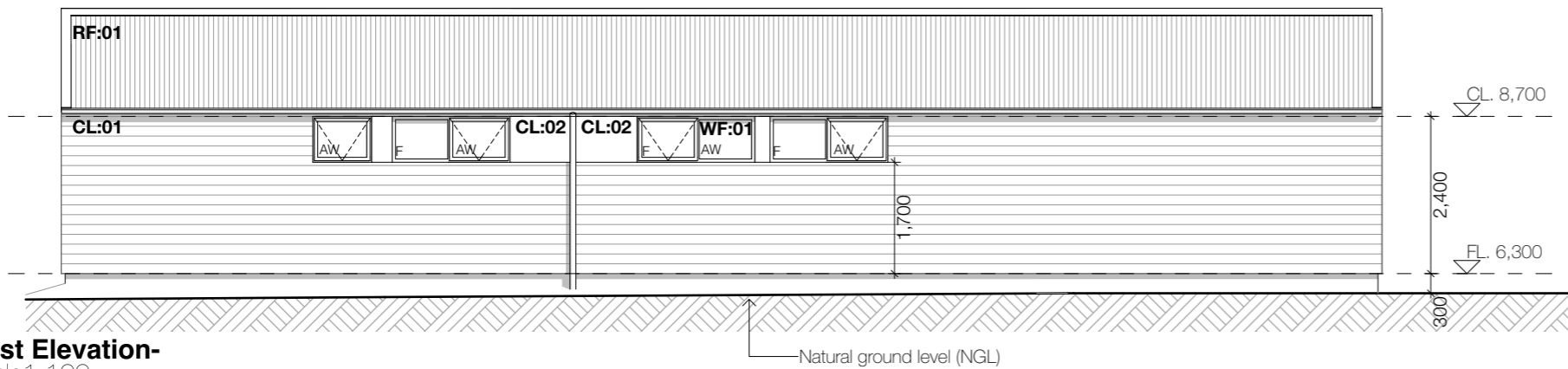
Elevation- Proposed Dwelling 3

scale 1:100



North Elevation-

scale 1:100



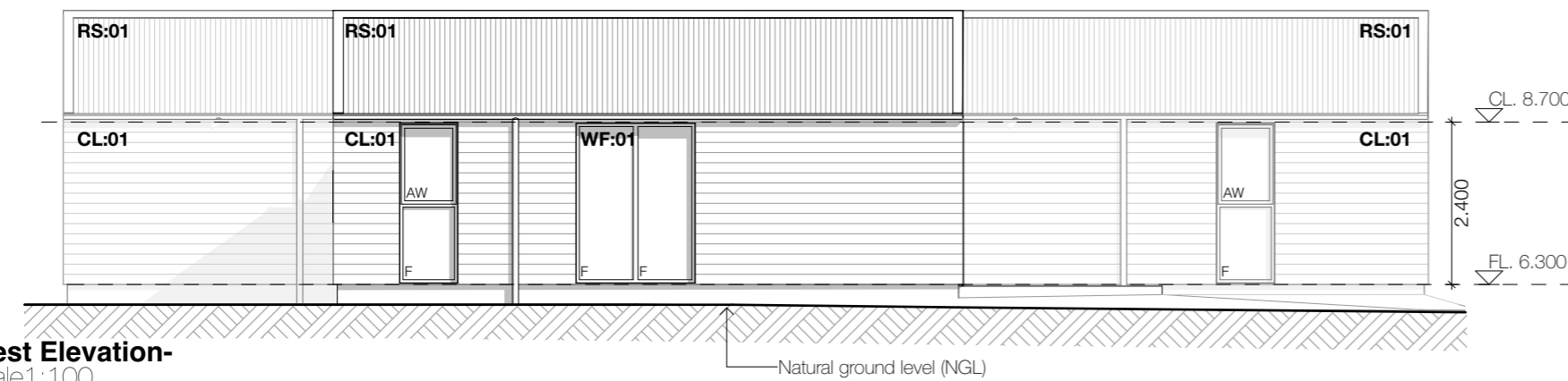
East Elevation-

scale 1:100



South Elevation-

scale 1:100



West Elevation-

scale 1:100

LEGEND

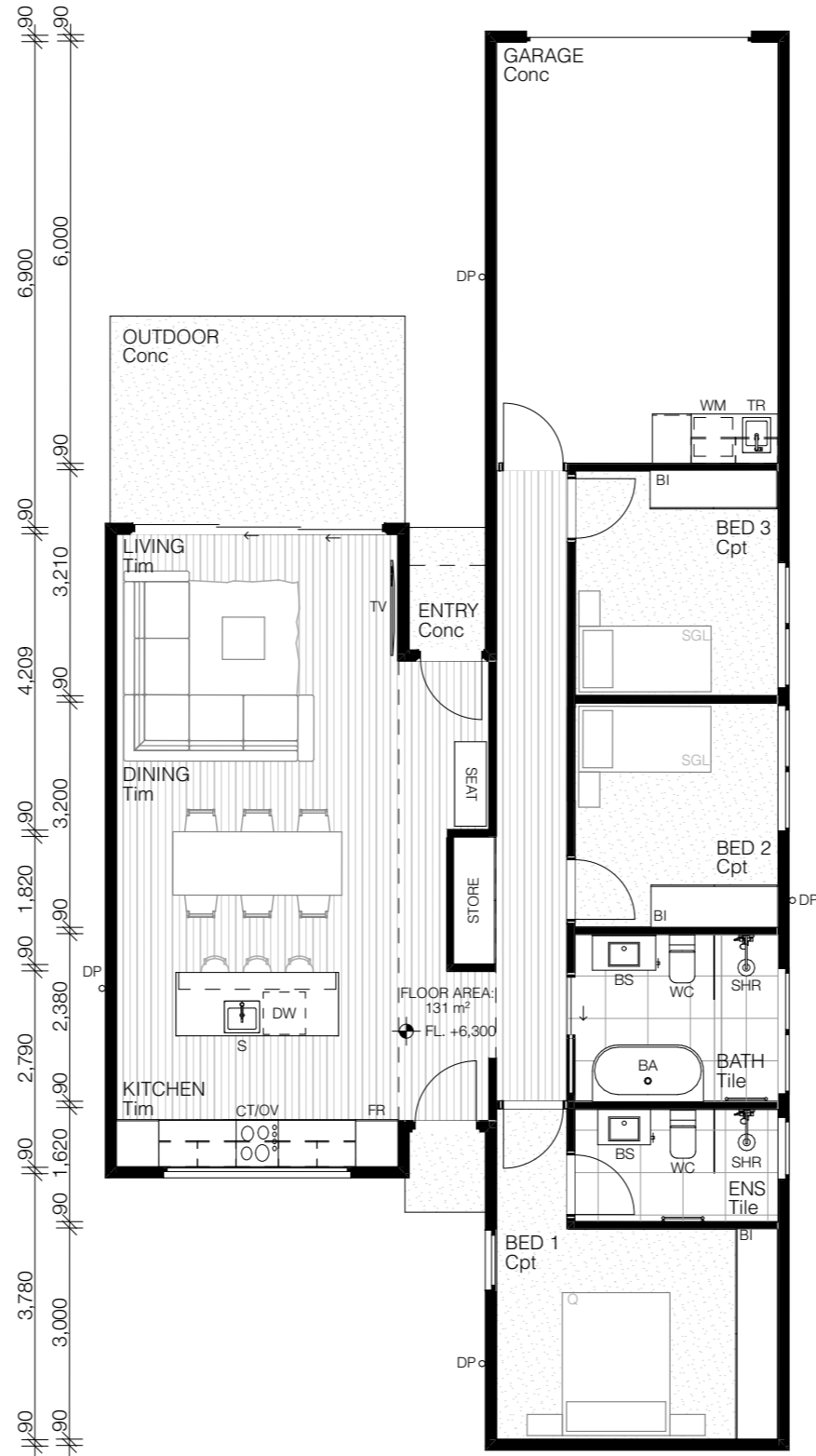
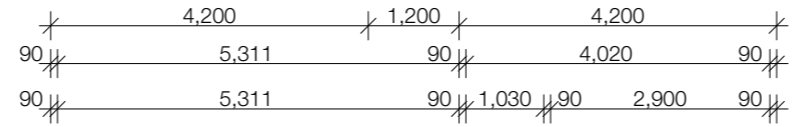
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Finish: Paint
- RS:01 Roofing
Deck: Corrugated metal 0.42bmt
Colour: Surfmist
- WF:01 Window and External Doors
Type: Double glazed aluminium frame
Glass Tint: TBC
Frame Colour: White



Floor Plan- Proposed Dwelling 4
scale 1:100



LEGEND

- █ Proposed wall
- ⊕ Existing levels
- ⊕ New levels
- FL - Finished floor level (top of particle board / slab)

Fixtures & Fittings

- AC Air Conditioner
- AC-OUT Air Conditioner- Outdoor Unit
- BI Built-in Cupboard
- BS Basin
- CJ Control joint
- CT Cooktop
- COL Column
- CT Cooktop
- CL Ceiling level
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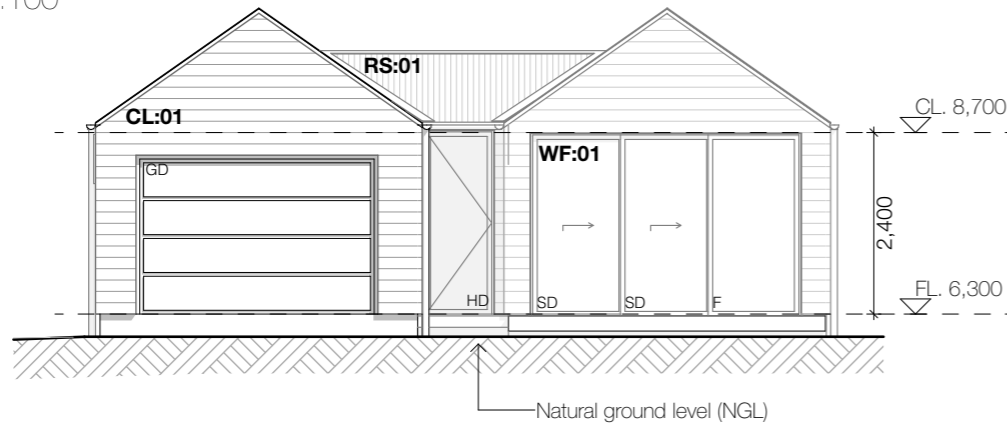
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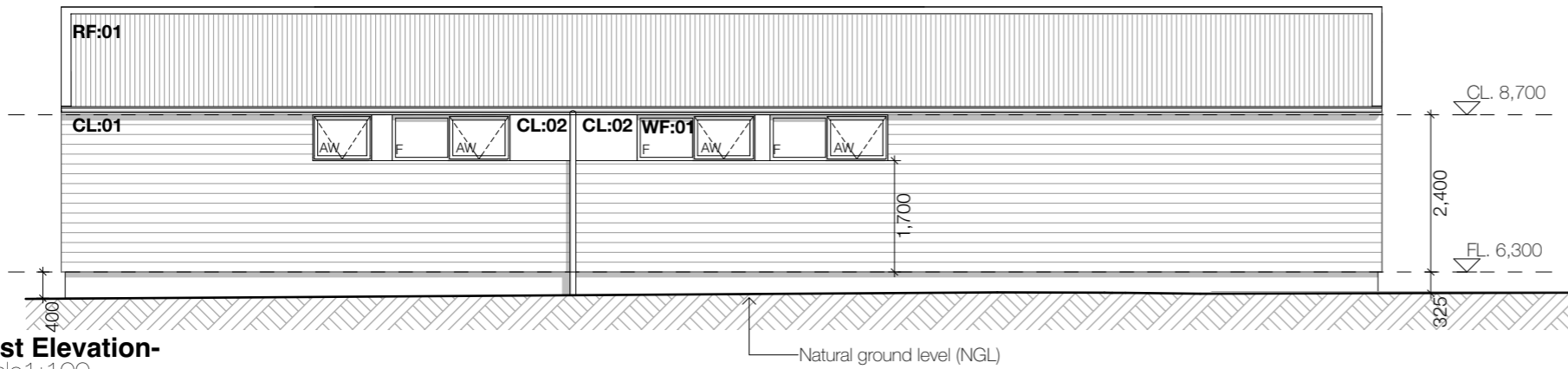
All works must be in accordance with relevant Australian Standards, the National Building Code, manufacturers specification and all other relevant regulatory bodies including local council.

Refer to Builder's scope of works for joinery inclusions.

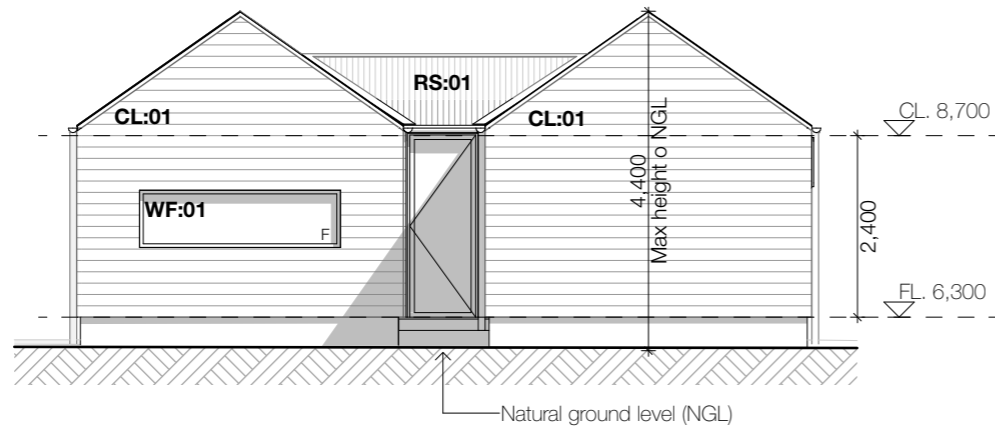
Elevation- Proposed Dwelling 4
scale 1:100



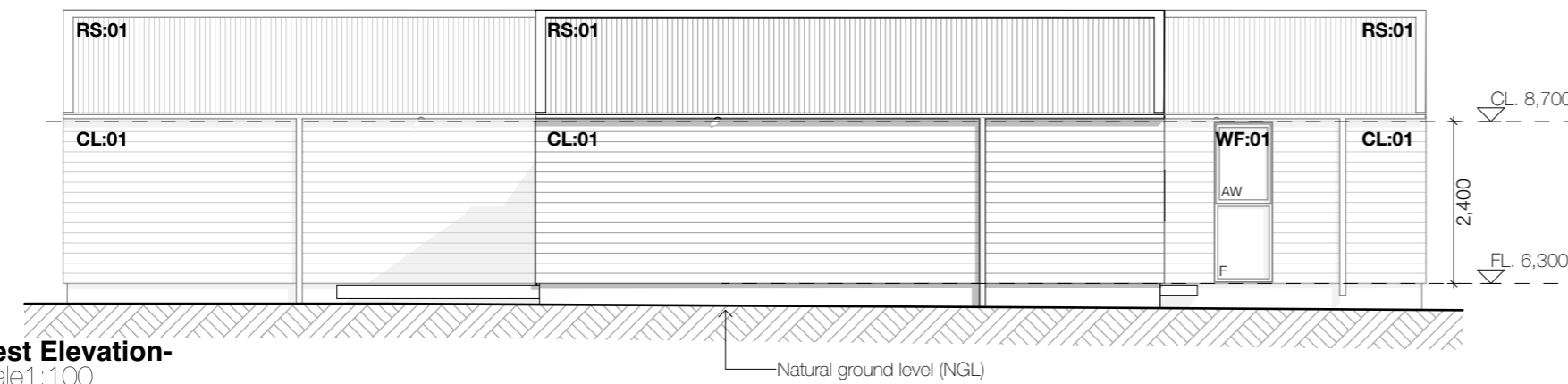
North Elevation-
scale 1:100



East Elevation-
scale 1:100



South Elevation-
scale 1:100



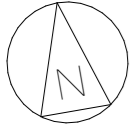
West Elevation-
scale 1:100

LEGEND

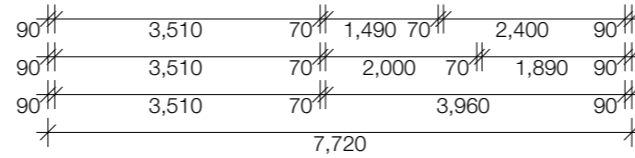
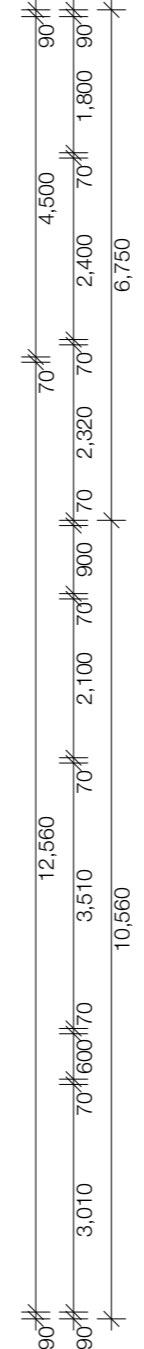
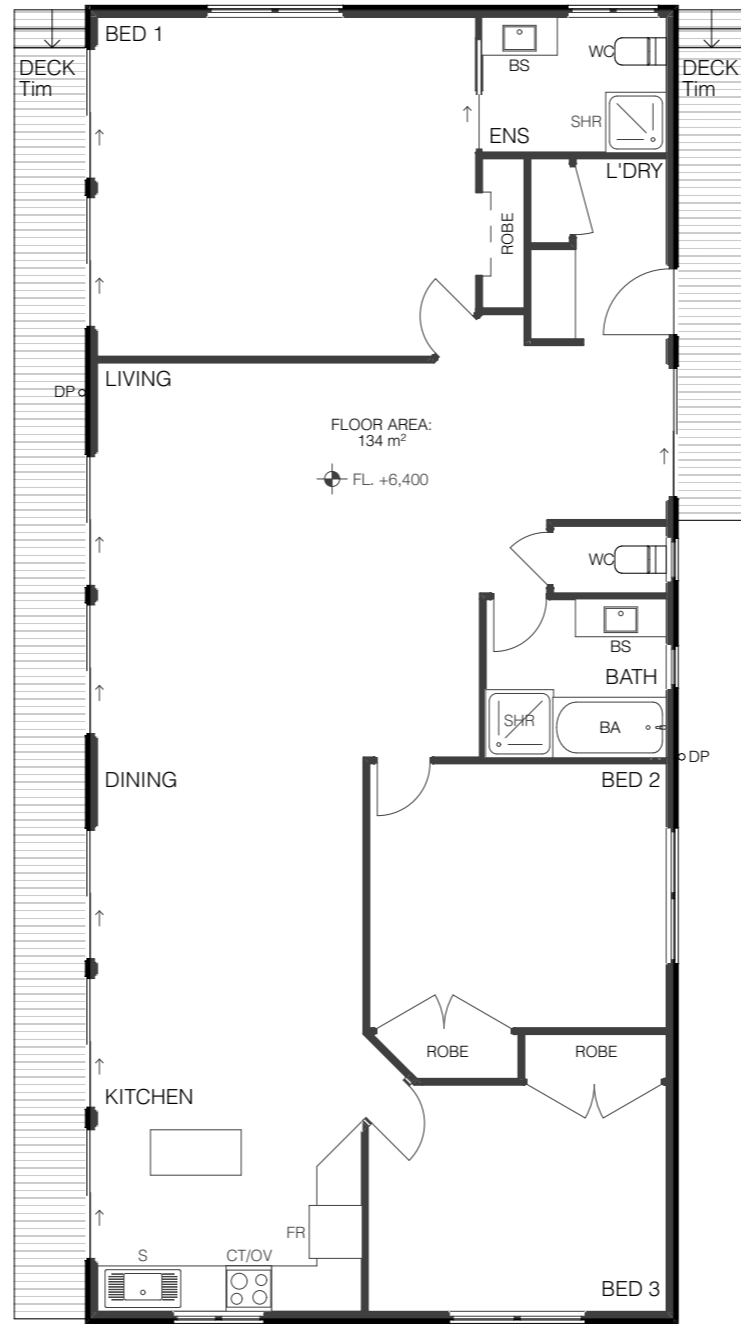
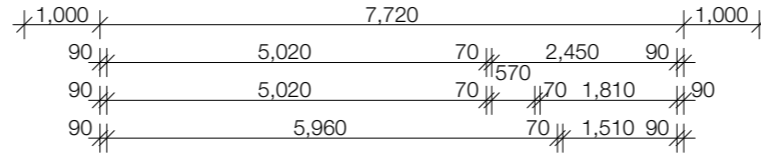
- AW - Awning window
- BB - Batten balustrade
- CJ - Control joint
- CL - Ceiling level
- DP - Downpipe
- F - Fixed window
- GB - Glass balustrade
- GD - Garage door
- HD - Glass door
- FL - Finished Floor level
- SD - Sliding door
- SW - Sliding window
- TAP - Water tap
- WB - Wire balustrade

SCHEDULE

- CL:01 Fibre Cement
Type: JH Linea
Colour: Surfmist
Finish: Paint
- RS:01 Roofing
Deck: Corrugated metal 0.42bmt
Colour: Surfmist
- WF:01 Window and External Doors
Type: Double glazed aluminium frame
Glass Tint: TBC
Frame Colour: White



Floor Plan- Proposed Dwelling 5
scale 1:100



LEGEND

- █ Proposed wall
- ⊕ Existing levels
- ⊕ New levels
- FL - Finished floor level (top of particle board / slab)

Fixtures & Fittings

- AC Air Conditioner
- AC-OUT Air Conditioner- Outdoor Unit
- BI Built-in Cupboard
- BS Basin
- CJ Control joint
- CT Cooktop
- COL Column
- CT Cooktop
- CL Ceiling level
- DP Down pipe
- FFL Finished Floor level
- FR Fridge
- FP Fire Pit
- GB Glass balustrade
- GD Garage Door
- GRD Grated Drain
- HWS Hot Water System
- LDRY Laundry
- MB Meter box
- NBN NBN Modem
- NBN-EXT NBN connection box
- OV Oven
- RWH Rainwater Head
- SHR Shower
- SK Skylight
- S Sink
- TAP Water Tap
- TR Trough
- TV Television
- WC Toilet
- WH Wood Heater
- WM Washing Machine
- WIR Walk-in Robe
- WIP Walk-in Pantry

Material/ Finish

- Conc Concrete paving slab.
- Cpt Carpet
- Paving Paving
- Scrn Screen
- Tile Floor tile
- Tim Plank flooring

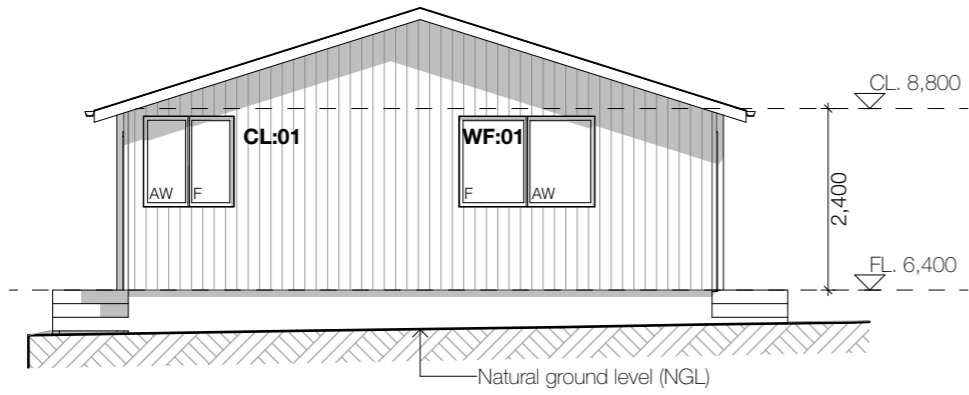
GENERAL NOTES

Finished landscaping must prevent water build up near footings, onto nearby structures and the driveway by providing sufficient fall and drainage systems in accordance with Australian Standards

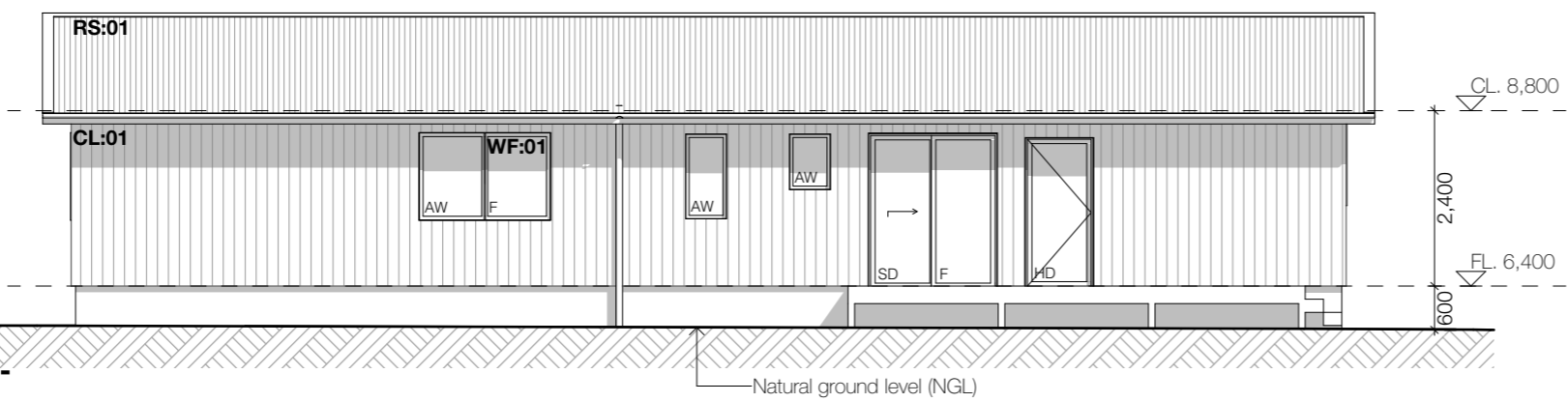
All works must be in accordance with relevant Australian Standards, the National Building Code, manufacturers specification and all other relevant regulatory bodies including local council.

Refer to Builder's scope of works for joinery inclusions.

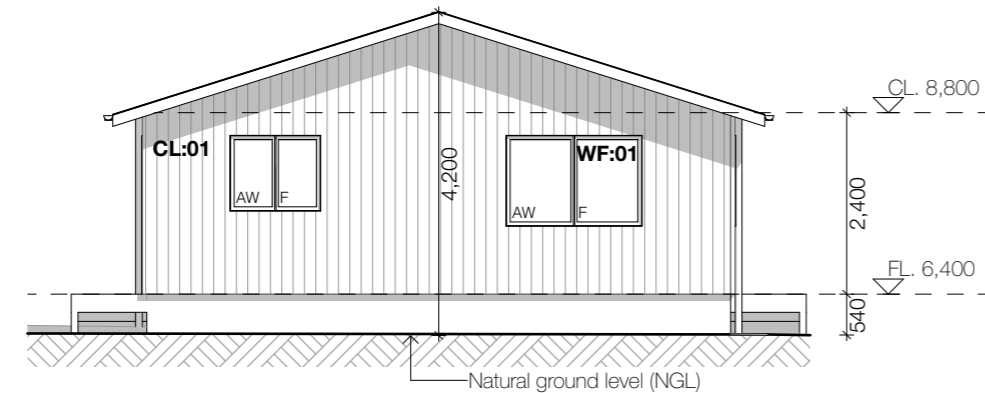
Elevation- Proposed Dwelling 5
scale 1:100



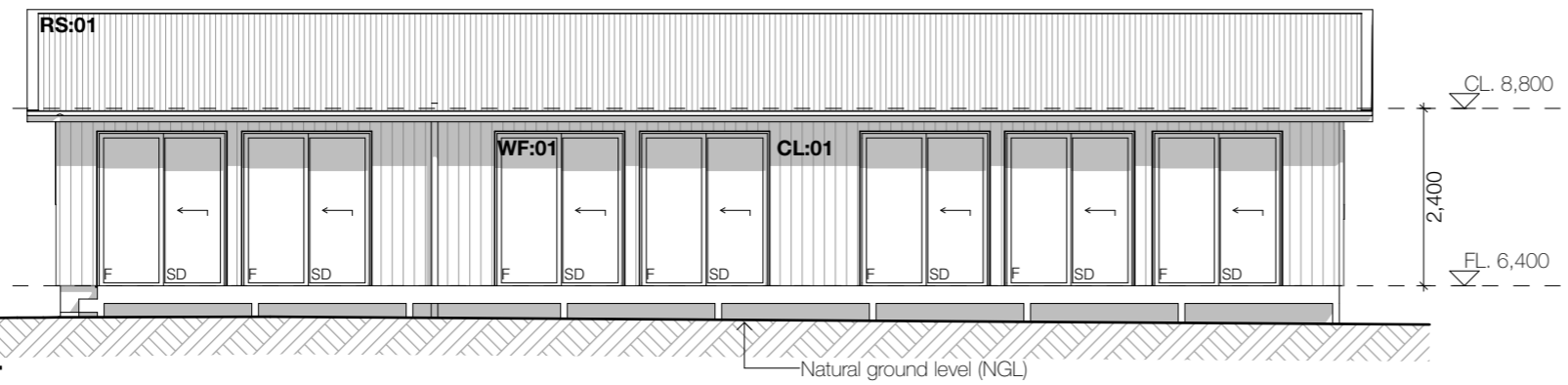
North Elevation-
scale 1:100



East Elevation-
scale 1:100



South Elevation-
scale 1:100



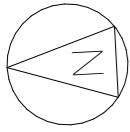
West Elevation-
scale 1:100

LEGEND

- AW - Awning window
- BB - Batten balustrade
- CJ - Control joint
- CL - Ceiling level
- DP - Downpipe
- F - Fixed window
- GB - Glass balustrade
- GD - Garage door
- HD - Glass door
- FL - Finished Floor level
- SD - Sliding door
- SW - Sliding window
- TAP - Water tap
- WB - Wire balustrade

SCHEDULE

- CL:01 Fibre Cement
Type: JH Linea
Colour: Surfmist
Finish: Paint
- RS:01 Roofing
Deck: Corrugated metal 0.42bmt
Colour: Surfmist
- WF:01 Window and External Doors
Type: Double glazed aluminium frame
Glass Tint: TBC
Frame Colour: White



Floor Plan- Existing Dwelling (6)
scale 1:100

LEGEND

- ▬ Proposed wall
- ⊕ Existing levels
- ⊕ New levels
- FL - Finished floor level (top of particle board / slab)

Fixtures & Fittings

- AC Air Conditioner
- AC-OUT Air Conditioner- Outdoor Unit
- BI Built-in Cupboard
- BS Basin
- CJ Control joint
- CT Cooktop
- COL Column
- CT Cooktop
- CL Ceiling level
- DP Down pipe
- FFL Finished Floor level
- FR Fridge
- FP Fire Pit
- GB Glass balustrade
- GD Garage Door
- GRD Grated Drain
- HWS Hot Water System
- LDRY Laundry
- MB Meter box
- NBN NBN Modem
- NBN-EXT NBN connection box
- OV Oven
- RWH Rainwater Head
- SHR Shower
- SK Skylight
- S Sink
- TAP Water Tap
- TR Trough
- TV Television
- WC Toilet
- WH Wood Heater
- WM Washing Machine
- WIR Walk-in Robe
- WIP Walk-in Pantry

Material/ Finish

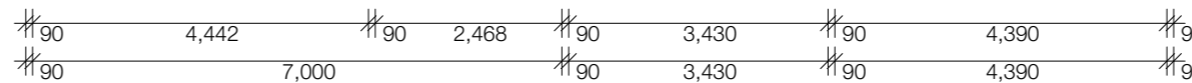
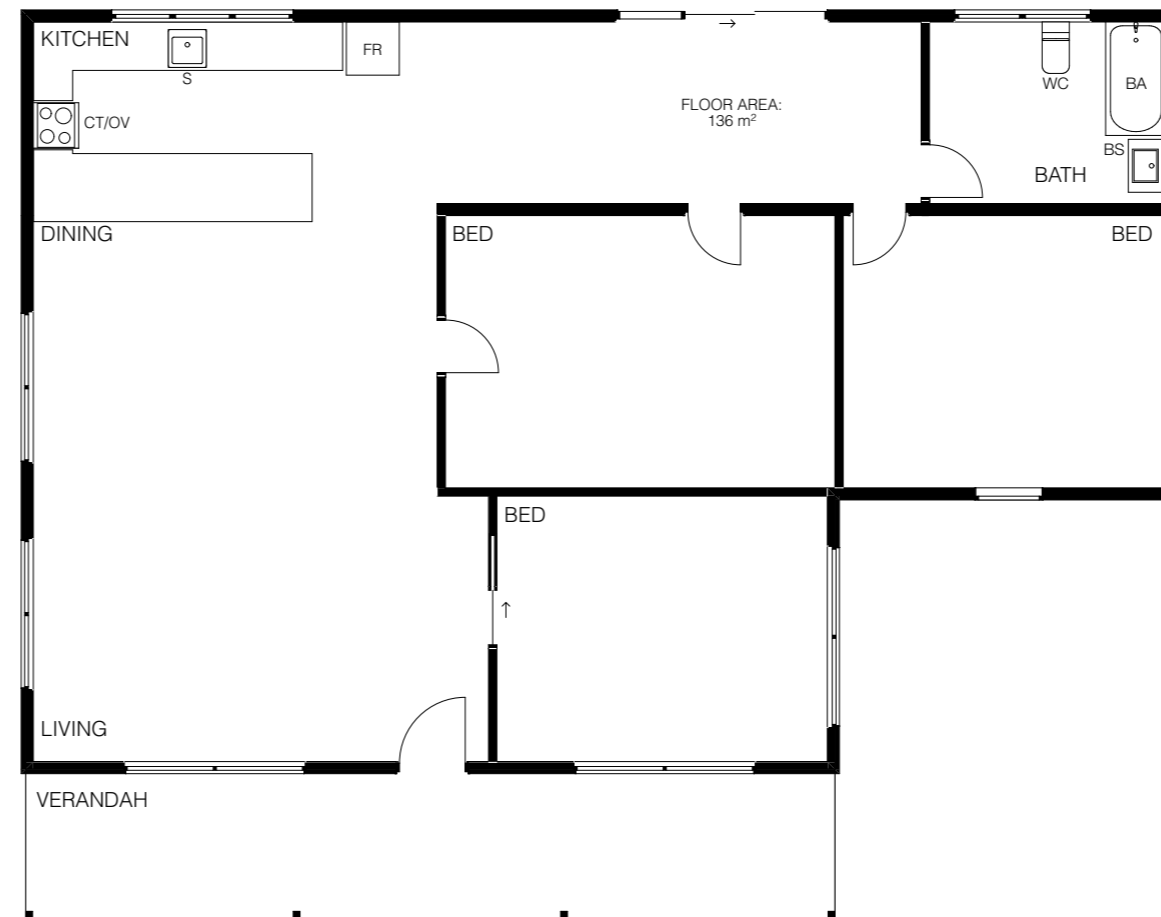
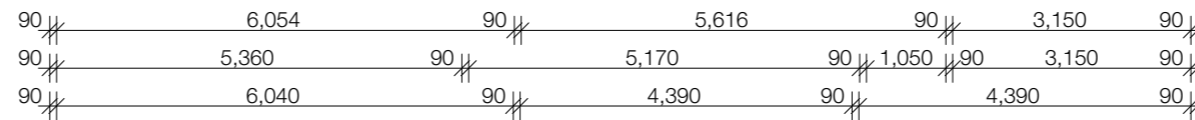
- Conc Concrete paving slab.
- Cpt Carpet
- Paving Paving
- Scrn Screen
- Tile Floor tile
- Tim Plank flooring

GENERAL NOTES

Finished landscaping must prevent water build up near footings, onto nearby structures and the driveway by providing sufficient fall and drainage systems in accordance with Australian Standards

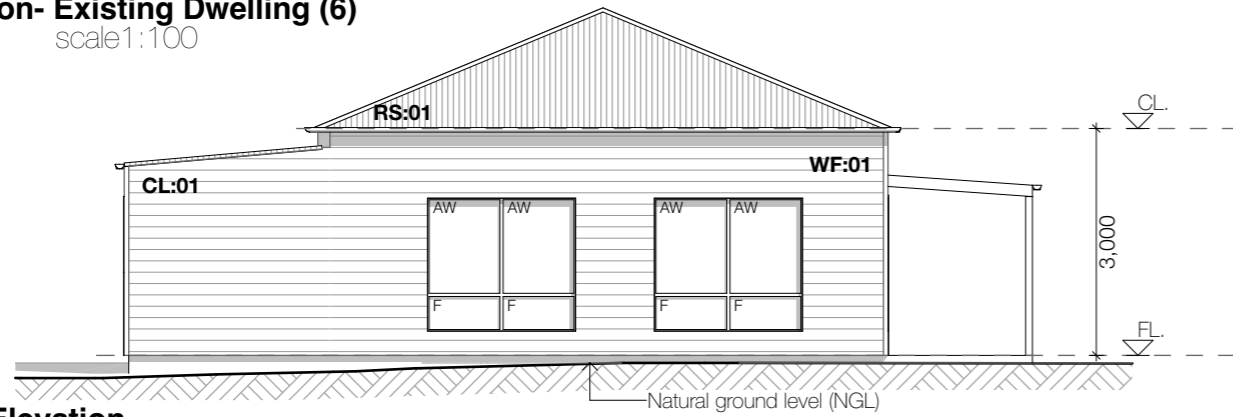
All works must be in accordance with relevant Australian Standards, the National Building Code, manufacturers specification and all other relevant regulatory bodies including local council.

Refer to Builder's scope of works for joinery inclusions.



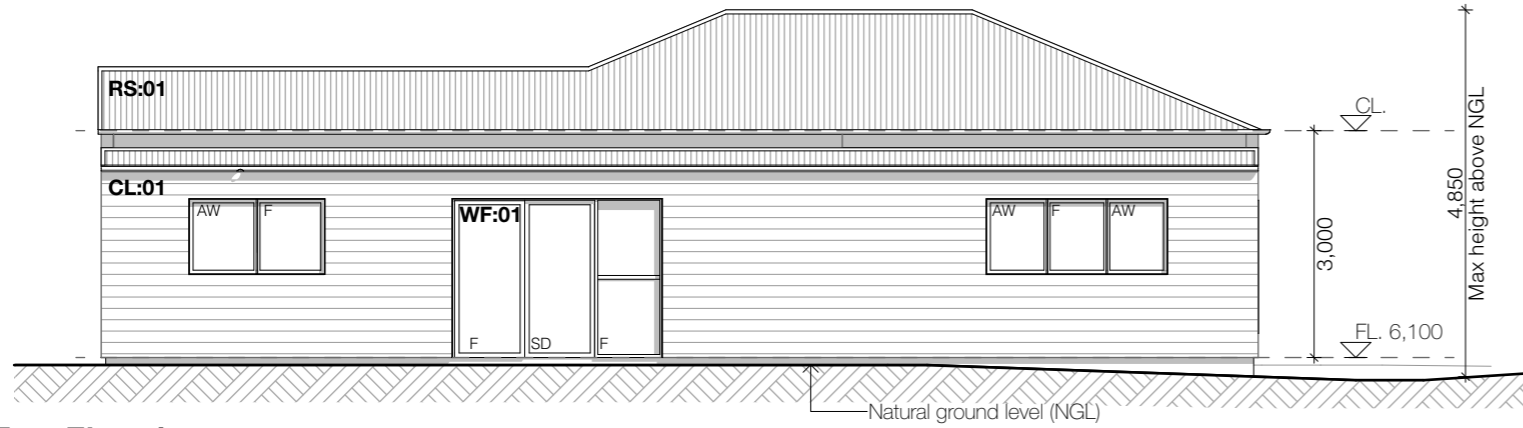
Elevation- Existing Dwelling (6)

scale 1:100



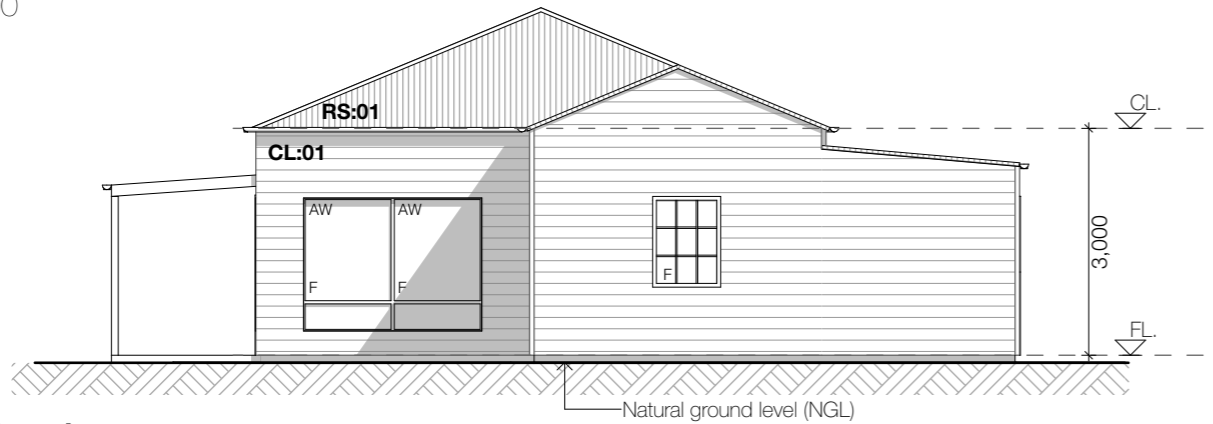
North Elevation-

scale 1:100



East Elevation-

scale 1:100



South Elevation-

scale 1:100



West Elevation-

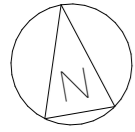
scale 1:100

LEGEND

- AW - Awning window
- BB - Batten balustrade
- CJ - Control joint
- CL - Ceiling level
- DP - Downpipe
- F - Fixed window
- GB - Glass balustrade
- GD - Garage door
- HD - Glass door
- FL - Finished Floor level
- SD - Sliding door
- SW - Sliding window
- TAP - Water tap
- WB - Wire balustrade

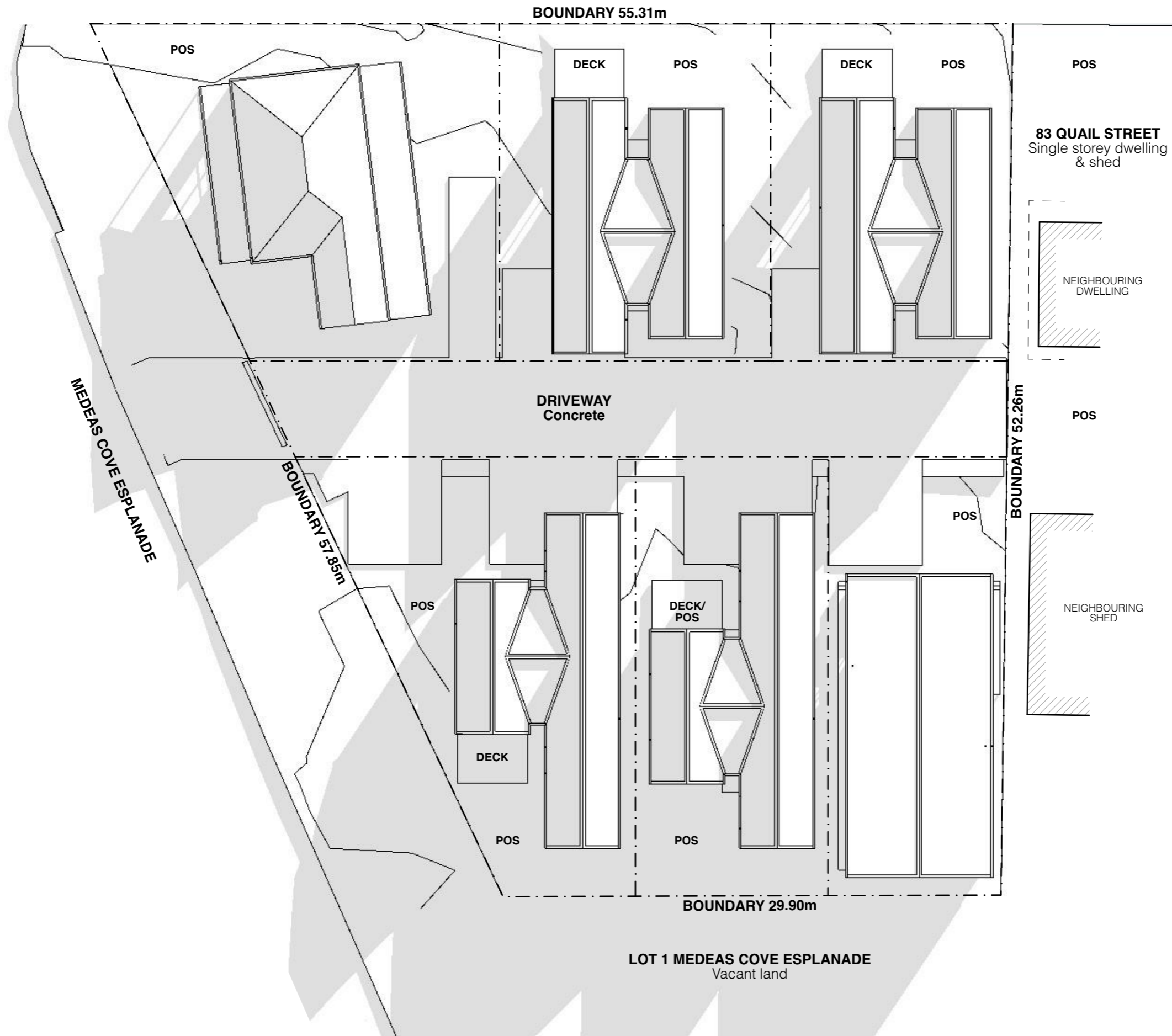
SCHEDULE

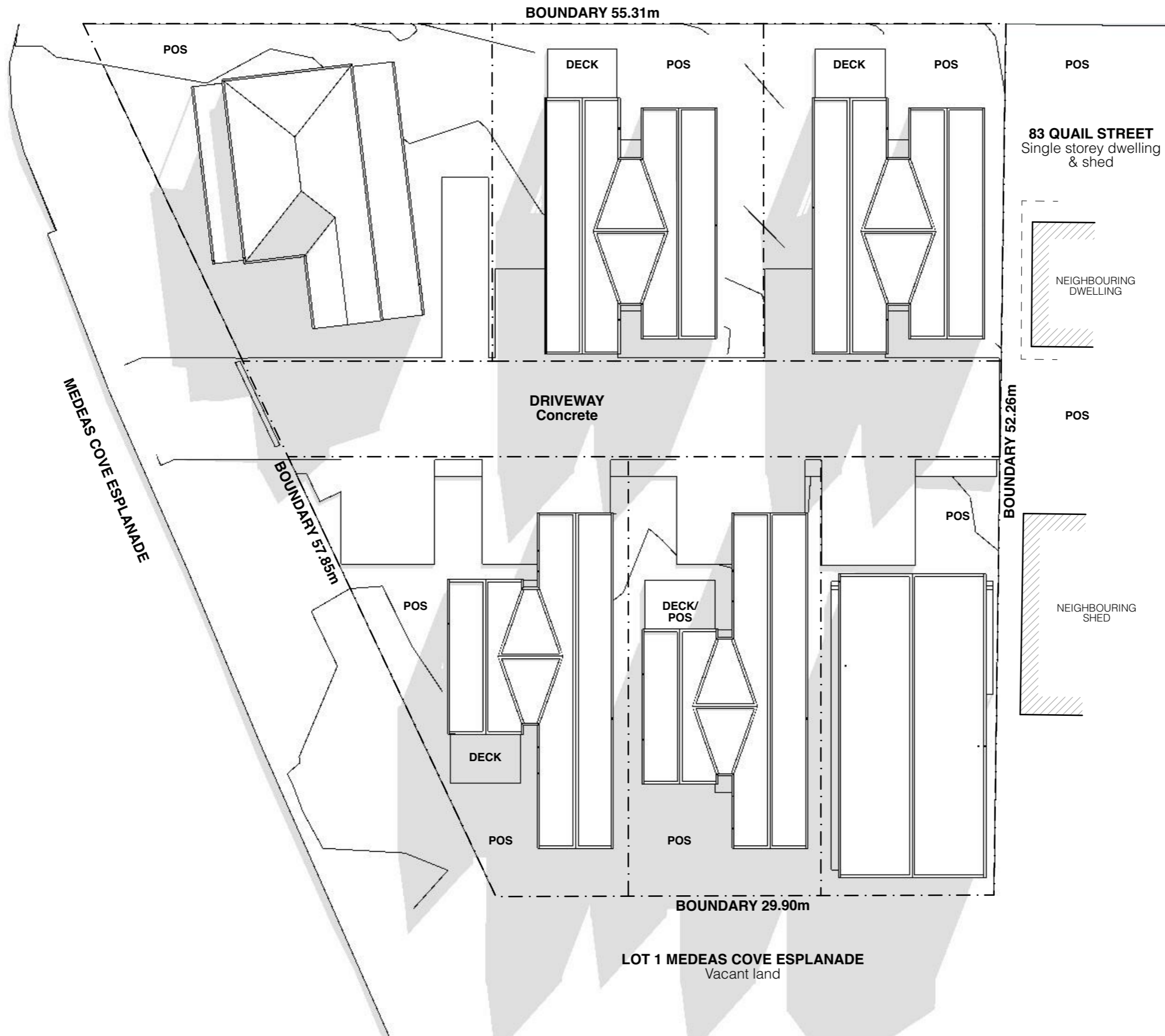
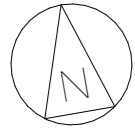
- CL:01 Weatherboard
Colour: White
Finish: Paint
- RS:01 Roofing
Deck: Corrugated metal 0.42bmt
Colour: Surfmist
- WF:01 Window and External Doors
Type: Double glazed aluminium frame
Glass Tint: TBC
Frame Colour: White



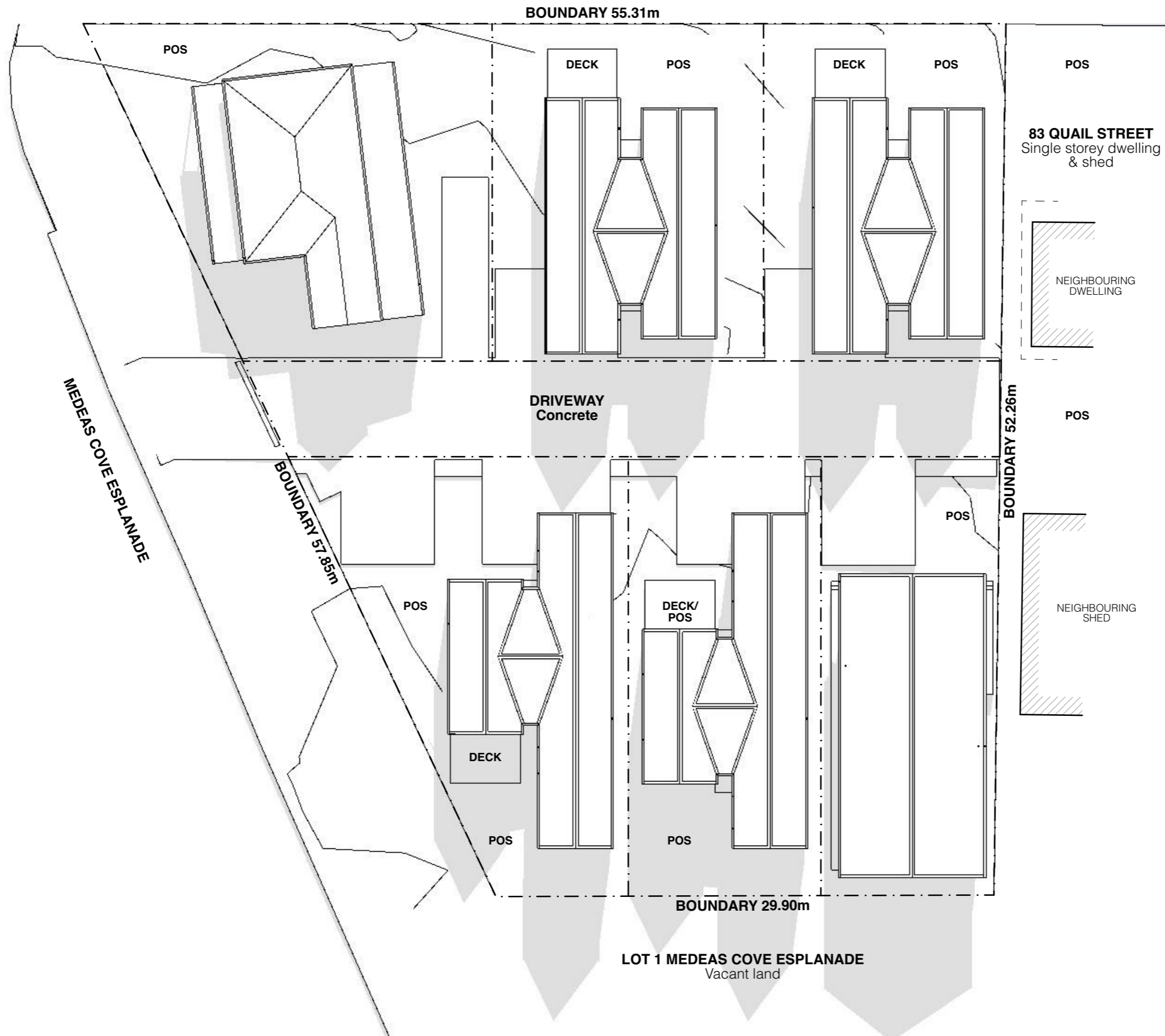
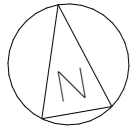
Shadow Diagrams- June 21- 9am
scale

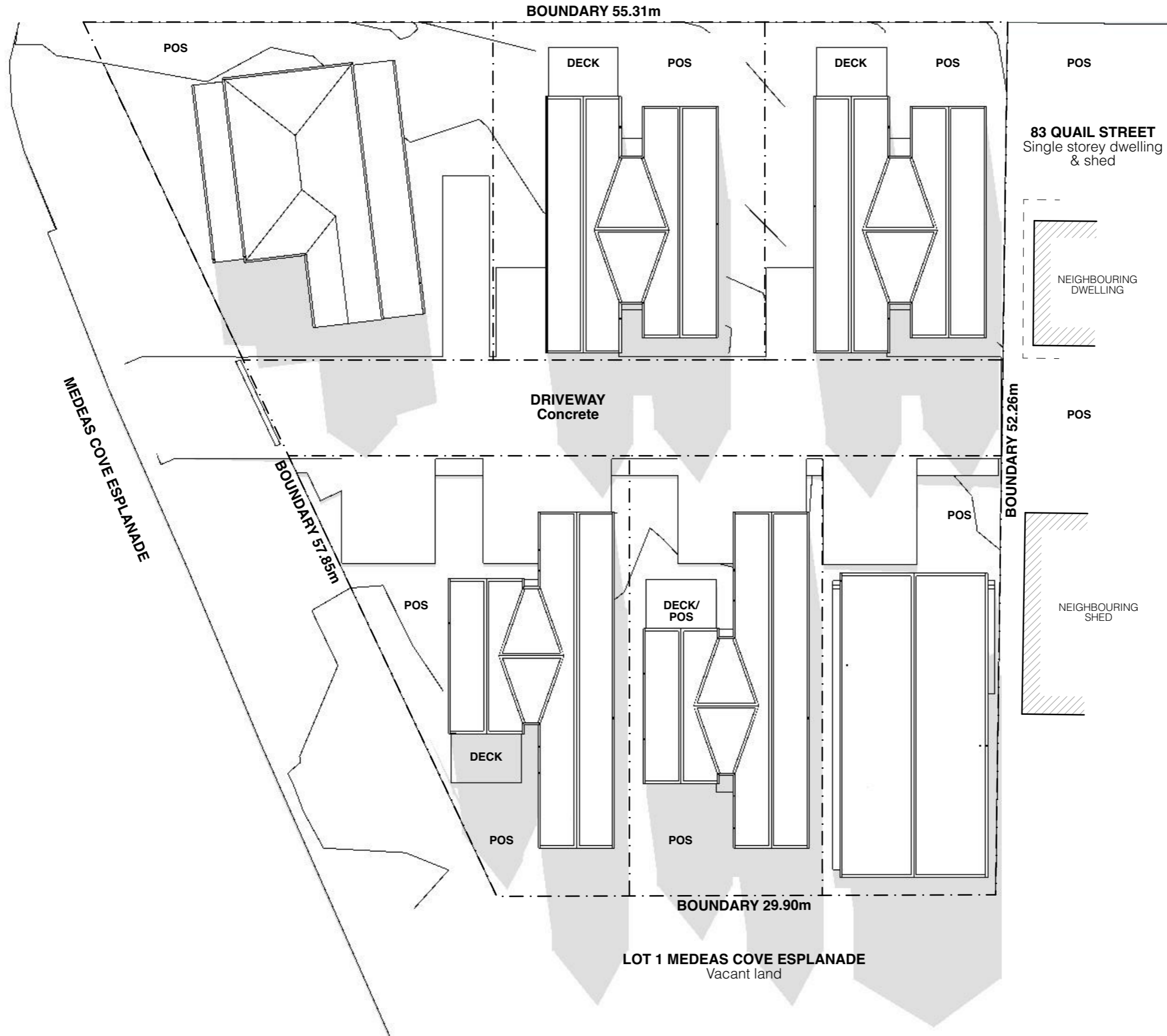
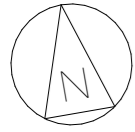
LEGEND
POS Private Open Space

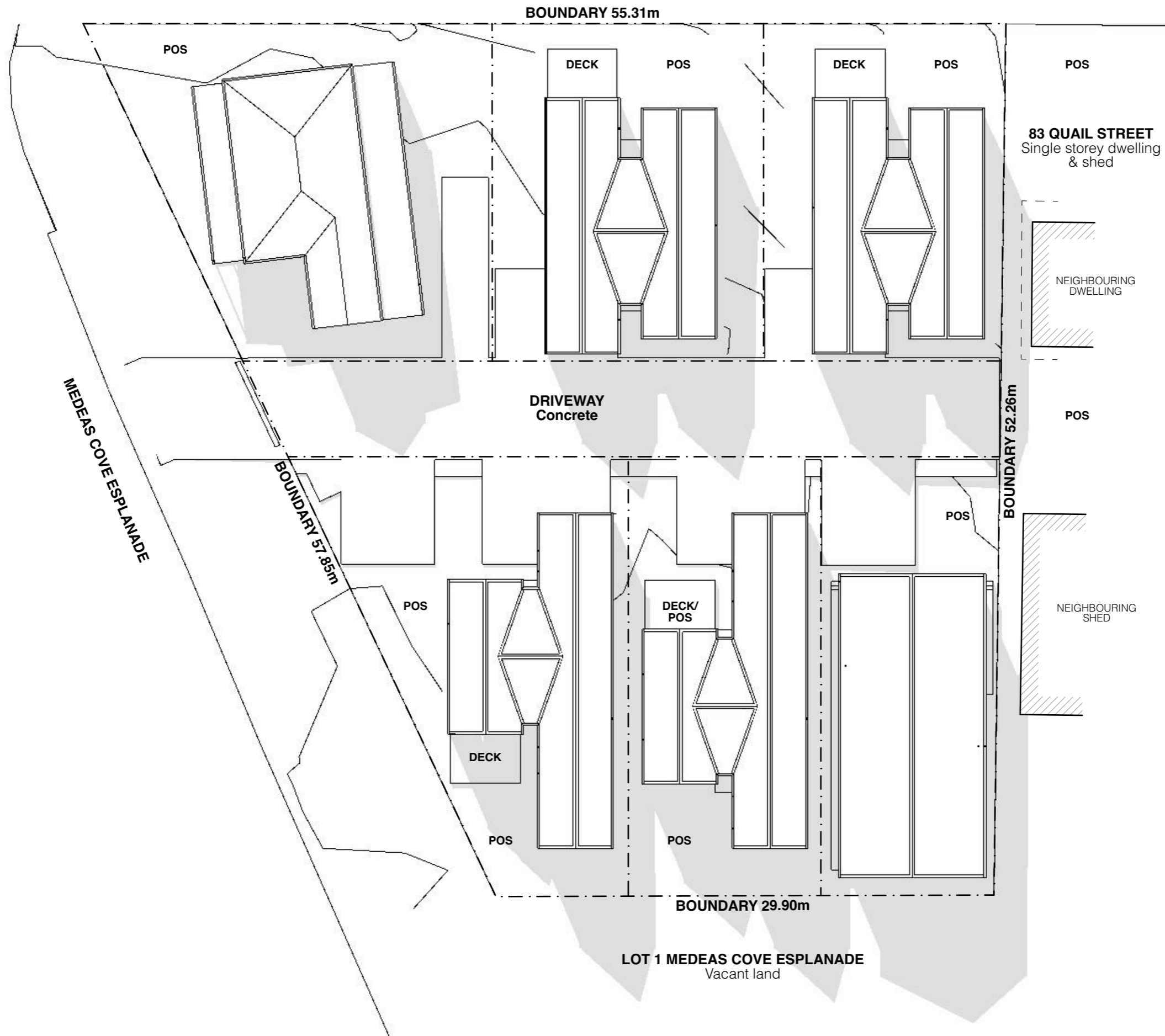
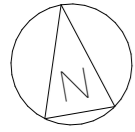


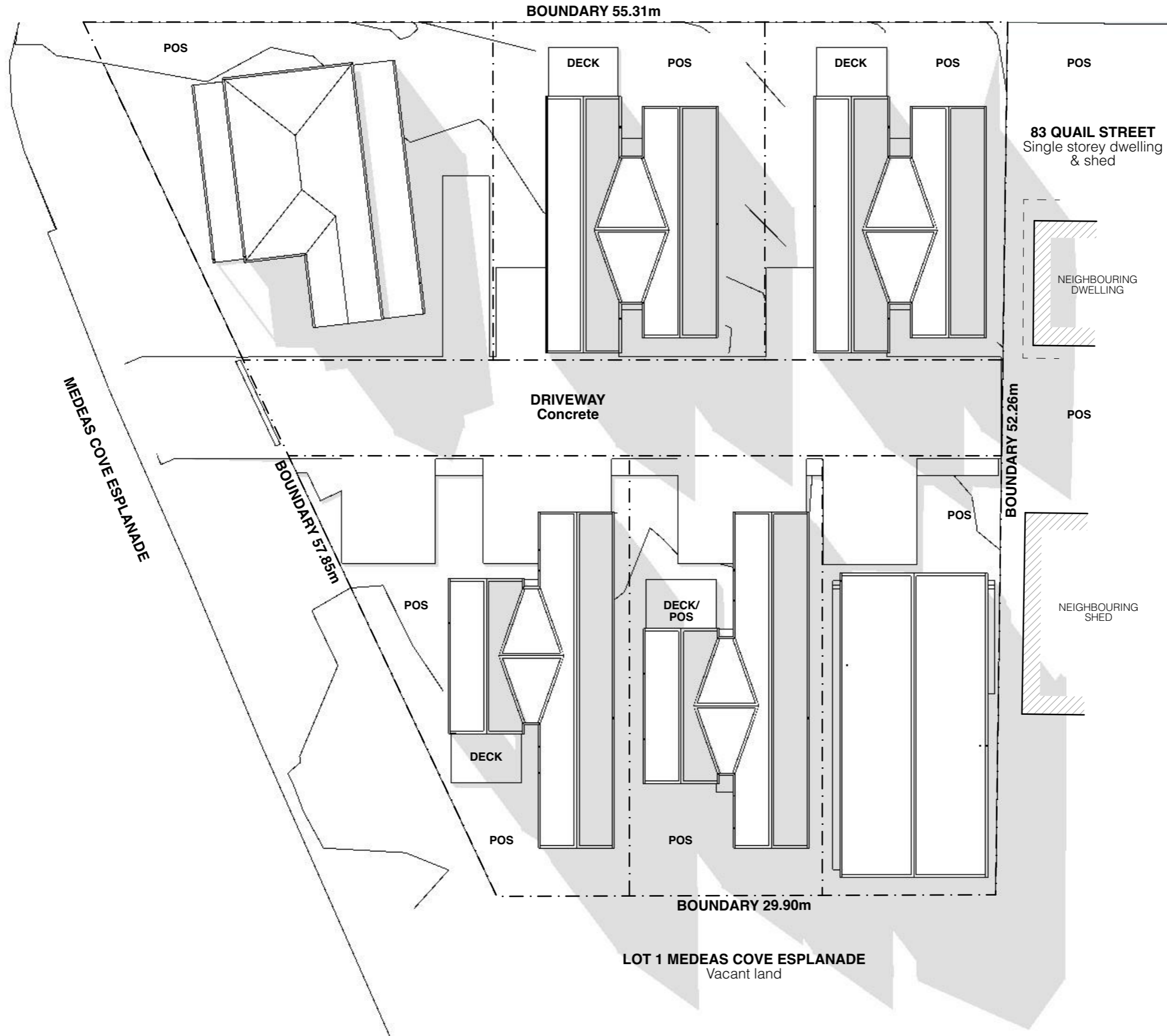
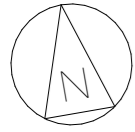


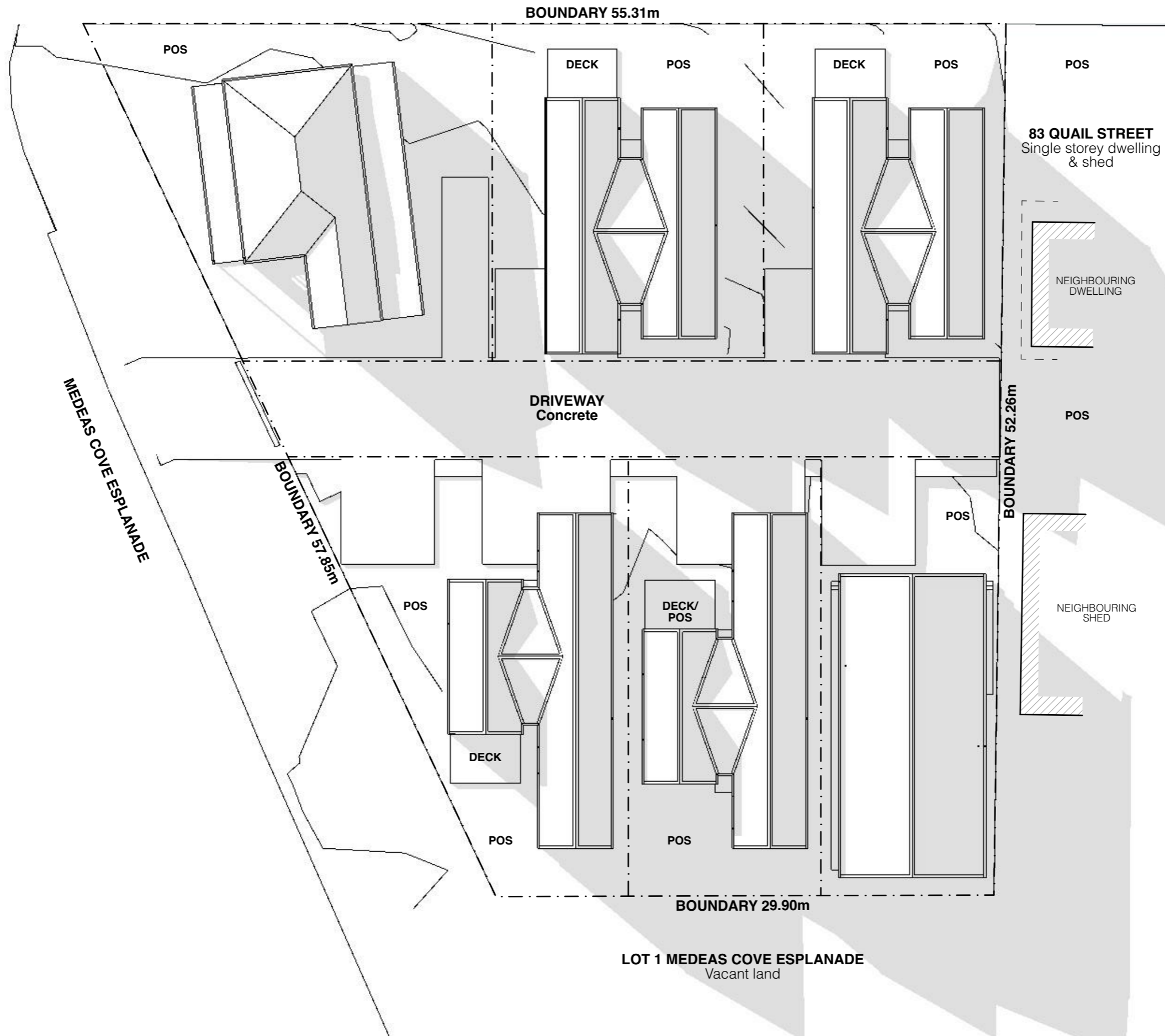
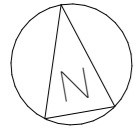
LOT 1 MEDEAS COVE ESPLANADE
Vacant land













Planning Response- 8.4.1 scale

COUNCIL REQUEST FOR INFORMATION

8.4.1 Residential density for multiple dwellings

Objective:

- That the density of multiple dwellings:
 (a) makes efficient use of land for housing; and
 (b) optimises the use of infrastructure and community services.

Performance Criteria

P1
 Multiple dwellings must only have a site area per dwelling that is less than 325m², if the development will not exceed the capacity of infrastructure services and:

- (a) is compatible with the density of existing development on established properties in the area;

Response

P1(a)
 Compliant. The proposed density is compatible as it is consistent with the established development pattern in the area, which is characterised by a mix of single dwellings, multi-dwelling developments, and small-scale commercial uses.

Existing properties in the area with a similar density, demonstrating compatibility with the surrounding development.



Site Plan- Density

Subject Site
 32 Medeas Cove Esplanade

COUNCIL REQUEST FOR INFORMATION

8.4.2 Setbacks and building envelope for all dwellings

Objective:

The siting and scale of dwellings:
(a) provides reasonably consistent separation between dwellings and their frontage within a street;

(b) provides consistency in the apparent scale, bulk, massing and proportion of dwellings;

(c) provides separation between dwellings on adjoining properties to allow reasonable opportunity for daylight and sunlight to enter habitable rooms and private open space; and

(d) provides reasonable access to sunlight for existing solar energy installations.

Performance Criteria

P1(a)

A dwelling must have a setback from a frontage that is compatible with the streetscape, having regard to any topographical constraints.

Response

P1(a)

Compliant. The front setback of the proposed dwellings is consistent with the existing dwelling on-site and existing dwellings on Quail Street. Refer 'Site Plan- Setbacks'

A2

A garage or carport for a dwelling must have a setback from a primary frontage of not less than:

(a) 5.5m, or alternatively 1m behind the building line;

(b) the same as the building line, if a portion of the dwelling gross floor area is located above the garage or carport; or

(c) 1m, if the existing ground level slopes up or down at a gradient steeper than 1 in 5 for a distance of 10m from the frontage.

Response

A2(a)

Compliant. The garage for a dwelling has a setback from a primary frontage of more than 5.5m. Refer 'Site Plan- Proposed'

(b) N/A

(c) N/A

Acceptable Solution

A3

A dwelling, excluding outbuildings with a building height of not more than 2.4m and protrusions that extend not more than 0.9m horizontally beyond the building envelope, must:

(a) be contained within a building envelope (refer to Figures 8.1, 8.2 and 8.3) determined by:

(i) a distance equal to the frontage setback or, for an internal lot, a distance of 4.5m from the rear boundary of a property with an adjoining frontage; and

Response

A3(a)(i)

The existing dwelling (6) and proposed dwelling 3 are outside the front setback building envelope. Refer 'Elevations- Site'. See below for compliance.

P3

The siting and scale of a dwelling must:
(a) not cause an unreasonable loss of amenity to adjoining properties, having regard to:

- (i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property;
- (ii) overshadowing the private open space of a dwelling on an adjoining property;
- (iii) overshadowing of an adjoining vacant property; and
- (iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property;

Response

P3(a)(i)(ii)(iii)(iv)

Compliant. The siting and scale of the existing dwelling (6) and proposed dwelling 3 do not cause an unreasonable loss of amenity to adjoining properties.

(b) provide separation between dwellings on adjoining properties that is consistent with that existing on established properties in the area; and

Response (b)

Compliant. The separation between dwellings is consistent with existing on established properties in the area- Refer Planning Response 8.4.1 Site Plan- Density

(c) not cause an unreasonable reduction in sunlight to an existing solar energy installation on:
(i) an adjoining property; or
(ii) another dwelling on the same site.

(c)(i)(ii)

Compliant. There are no existing solar energy installations on existing properties.

A3

(i) projecting a line at an angle of 45 degrees from the horizontal at a height of 3m above existing ground level at the side and rear boundaries to a building height of not more than 8.5m above existing ground level; and

Response

A3(a)(ii)

Compliant. The proposed dwellings 1, 2, 4 and 5 are within the side and rear building envelope. Refer Elevations- Site.

(b) only have a setback of less than 1.5m from a side or rear boundary if the dwelling:

(i) does not extend beyond an existing building built on or within 0.2m of the boundary of the adjoining property; or

Response

A3(b)(i)

Compliant. There are no adjoining dwellings that are built on or within 0.2m of the boundary of the adjoining property. Refer Site Plan- Proposed

(ii) does not exceed a total length of 9m or one third the length of the side boundary (whichever is the lesser).

Response

A3(b)(ii)

N/A

8.4.3 Site coverage and private open space for all dwellings

Objective:

That dwellings are compatible with the amenity and character of the area and provide:
(a) for outdoor recreation and the operational needs of the residents;
(b) opportunities for the planting of gardens and landscaping; and
(c) private open space that is conveniently located and has access to sunlight.

Acceptable Solutions

A1

Dwellings must have:
(a) a site coverage of not more than 50% (excluding eaves up to 0.6m wide); and

(b) for multiple dwellings, a total area of private open space of not less than 60m² associated with each dwelling, unless the dwelling has a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer).

Response

A1(a)

Compliant. The site coverage is less than 50%. Refer to Cover Page.

A1(b)

Compliant. The private open space associated with each dwelling is more than 60m². Refer Site Plan- Proposed.

A2

A dwelling must have private open space that:

(a) is in one location and is not less than:

(i) 24m²; or

(ii) 12m², if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer);

Response

A2(a)

Compliant. The proposed dwellings have private open space more than 24m² in one location. Refer Site Plan- Proposed.

(b) has a minimum horizontal dimension of not less than:

(i) 4m; or

(ii) 2m, if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer);

Response

A2(b)

Compliant. The proposed dwellings private open space has a minimal horizontal distance of 4m. Refer Site Plan- Proposed.

(c) is located between the dwelling and the frontage only if the frontage is orientated between 30 degrees west of true north and 30 degrees east of true north; and

(d) has a gradient not steeper than 1 in 10.

Response

A2(c)

Compliant. Refer Site Plan- Proposed.

(d)

Compliant. The gradient of the private open space is less than 1 in 10.

8.4.4 Sunlight to private open space of multiple dwellings

Objective:

That the separation between multiple dwellings provides reasonable opportunity for sunlight to private open space for dwellings on the same site.

A1

A multiple dwelling, that is to the north of the private open space of another dwelling on the same site, required to satisfy A2 or P2 of clause 8.4.3, must satisfy (a) or (b), unless excluded by (c):

(a) the multiple dwelling is contained within a line projecting (see Figure 8.4):

(i) at a distance of 3m from the northern edge of the private open space; and

(ii) vertically to a height of 3m above existing ground level and then at an angle of 45 degrees from the horizontal;

(b) the multiple dwelling does not cause 50% of the private open space to receive less than 3 hours of sunlight between 9.00am and 3.00pm on 21st June; and

Response

A1(b)

Compliant. Refer to Shadow Diagrams.

(c) this Acceptable Solution excludes that part of a multiple dwelling consisting of:

(i) an outbuilding with a building height not more than 2.4m; or

(ii) protrusions that extend not more than 0.9m horizontally from the multiple dwelling.

8.4.6 Privacy for all dwellings

Objective:

To provide a reasonable opportunity for privacy for dwellings.

A1

A balcony, deck, roof terrace, parking space, or carport for a dwelling (whether freestanding or part of the dwelling), that has a finished surface or floor level more than 1m above existing ground level must have a permanently fixed screen to a height of not less than 1.7m above the finished surface or floor level, with a uniform transparency of not more than 25%, along the sides facing a:

(a) side boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of not less than 3m from the side boundary;

(b) rear boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of not less than 4m from the rear boundary; and

(c) dwelling on the same site, unless the balcony, deck, roof terrace, parking space, or carport is not less than 6m:

(i) from a window or glazed door, to a habitable room of the other dwelling on the same site; or

(ii) from a balcony, deck, roof terrace or the private open space of the other dwelling on the same site.

Response

A1

N/A. There are no finished surfaces or floor levels that are more than 1m above existing ground level.

A2

A window or glazed door to a habitable room of a dwelling, that has a floor level more than 1m above existing ground level, must satisfy (a), unless it satisfies (b):

(a) the window or glazed door:

(i) is to have a setback of not less than 3m from a side boundary;

(ii) is to have a setback of not less than 4m from a rear boundary;

(iii) if the dwelling is a multiple dwelling, is to be not less than 6m from a window or glazed door, to a habitable room, of another dwelling on the same site; and

(iv) if the dwelling is a multiple dwelling, is to be not less than 6m from the private open space of another dwelling on the same site.

(b) the window or glazed door:

(i) is to be offset, in the horizontal plane, not less than 1.5m from the edge of a window or glazed door, to a habitable room of another dwelling;

(ii) is to have a sill height of not less than 1.7m above the floor level or have fixed obscure glazing extending to a height of not less than 1.7m above the floor level; or

(iii) is to have a permanently fixed external screen for the full length of the window or glazed door, to a height of not less than 1.7m above floor level, with a uniform transparency of not more than 25%.

COUNCIL REQUEST FOR INFORMATION

8.4.8 Waste storage for multiple dwellings

Objective:

To provide for the storage of waste and recycling bins for multiple dwellings.

Acceptable Solution

A1

A multiple dwelling must have a storage area, for waste and recycling bins, that is not less than 1.5m² per dwelling and is within one of the following locations:

(a) an area for the exclusive use of each dwelling, excluding the area in front of the dwelling; or

Response

A1(a)

The proposed dwellings provide an area of 1.5m² to the side of each dwelling.

(b) a common storage area with an impervious surface that:

(i) has a setback of not less than 4.5m from a frontage;

(ii) is not less than 5.5m from any dwelling; and

(iii) is screened from the frontage and any dwelling by a wall to a height not less than 1.2m above the finished surface level of the storage area.

C7.6.1 Buildings and works within a waterway and coastal protection area or a future coastal refugia area

Objective:

That buildings and works within a waterway and coastal protection area or future coastal refugia area will not have an unnecessary or unacceptable impact on natural assets.

Performance Criteria

P1.1

Buildings and works within a waterway and coastal protection area must avoid or minimise adverse impacts on natural assets, having regard to:

(a) impacts caused by erosion, siltation, sedimentation and runoff;

Response

There are no signs of erosion, siltation, sedimentation and runoff within the property.

(b) impacts on riparian or littoral vegetation;

Response

There is no riparian or littoral vegetation within the property.

(c) maintaining natural streambank and streambed condition, where it exists;

Response

There are no natural streambanks or streambeds within the property.

(d) impacts on in-stream natural habitat, such as fallen logs, bank overhangs, rocks and trailing vegetation;

Response

Not applicable- see above.

(e) the need to avoid significantly impeding natural flow and drainage;

Response

Not applicable- see above.

(f) the need to maintain fish passage, where known to exist;

Response

Not applicable- see above.

(g) the need to avoid land filling of wetlands;

Response

Not applicable- see above.

(h) the need to group new facilities with existing facilities, where reasonably practical;

Response

The proposal is for a new single dwelling.

(i) minimising cut and fill;

Response

Cut and fill has been minimised on-site.

(j) building design that responds to the particular size, shape, contours or slope of the land;

Response

The siting of the dwelling is compatible with the streetscape and character of development existing on established properties in the area

(k) minimising impacts on coastal processes, including sand movement and wave action;

Response

N/A

(l) minimising the need for future works for the protection of natural assets, infrastructure and property;

(m) the environmental best practice guidelines in the Wetlands and Waterways Works Manual; and

(n) the guidelines in the Tasmanian Coastal Works Manual.

Response

The proposal minimises adverse effects on natural assets.

BRE-S2.7.1 Stormwater management

Objective:

That development provides for adequate stormwater management.

Acceptable Solution

A1

Development must be:

(a) capable of connecting to the public stormwater system; or

(b) permitted by the General Manager to discharge stormwater to a system other than the public stormwater system.

Response

A1(a)

The proposed dwellings and driveway connect to the public stormwater infrastructure.



32 Medeas Cove Esp, St Helens Multiple Dwelling Development

TRAFFIC IMPACT ASSESSMENT

- Draft
- March 2026

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Contents

1. Introduction	5
1.1 Background	5
1.2 Objectives	5
1.3 Scope of Traffic Impact Assessment (TIA)	5
1.4 References	5
2. Site Description	9
3. Development Proposal & Planning Scheme	10
3.1 Description of Proposed Development	10
3.2 Council Planning Scheme	11
3.3 Council Road Network Objectives	11
4. Existing Conditions	12
4.1 Transport Network	12
4.2 Medeas Cove Esplanade	12
4.3 Quail Street	12
4.4 Medeas Cove / Quail Street junction	12
4.5 Proposed 32 Medeas Cove Esplanade access	14
4.6 Sight Distance Summary (Figure 17)	16
4.7 Traffic Activity (AADT)	17
4.8 Crash History	17
4.9 Medeas Cove Esp. / Quail St. & approaches Road Safety Review	18
4.10 Safe Systems Assessment	18
5. Traffic Generation and Assignment	19
5.1 Traffic Growth	19
5.2 Trip Generation	19
5.3 Trip Assignment	19
6. Impact on Road Network	21
6.1 Impact of traffic generated by the proposal	21
6.2 32 Medeas Cove Property access	21
6.3 Medeas Cove Esplanade / Quail Street Junction	21
6.4 Austroads Junction Warrant	21
6.5 Tasmanian Planning Scheme – Break O Day	22
6.5.1 Parking and Sustainable Transport Code C2	22
6.5.2 Road and Railway Assets Code C3	27
6.6 Other requirements	28
6.6.1 Environmental	28
6.6.2 Street Lighting and Furniture	28
6.7 Property access standard	28



6.8	Tasmanian Subdivision Guideline Considerations	30
6.9	Transport Planning Considerations	30
6.10	Provisions for all road users	30
6.10.1	Light Vehicles	30
6.10.2	Garbage Collection	30
6.10.3	Public Transport	30
6.10.4	Vulnerable Road Users	30
6.11	Services	30
7.	Recommendations and Conclusions	31
	Appendices	32
	Appendix A - Level of Service Descriptions	33
	Appendix B - Safe Systems Assessment	34
	Appendix C - Tas. 26m B Double Network	35
	Appendix D - Development Design Plans	36
	Appendix E – Rural Road Property access	46



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1. Introduction

1.1 Background

In accordance with Tasmanian Planning Scheme – Break O Day a Traffic Impact Assessment (TIA) is required to be submitted with the development application for the existing 3-bedroom house and proposed 5*3-bedroom unit multiple dwelling development at 32 Medeas Cove Esplanade, St Helens. The TIA must provide details as follows:

- Anticipated additional traffic and pedestrian movements.
- The significance of the impact of these movements on the existing road network
- Any changes required to accommodate the additional traffic.

The TIA has been prepared based on Department of State Growth (DSG) guidelines.

1.2 Objectives

A Traffic Impact Assessment is a means for assisting in the planning and design of sustainable development proposals that consider:

- Safety
- Capacity
- Equity and social justice
- Economic efficiency
- The environment
- Future development

This report considers traffic projections to 10 years beyond the opening of the development.

1.3 Scope of Traffic Impact Assessment (TIA)

This TIA considers in detail the impact of the proposal on Medeas Cove Esp. and Quail St and the proposed property access relocation from Quail Street to Medeas Cove Esp.

1.4 References

- NSW Guide to Transport Impact Assessment 2024
- Tasmanian Planning Scheme – Break O Day
- Austroads Guide Road Design Part 4A: Unsignalized & Signalised Intersections 2023
- Guide to Traffic Management Part 6: Intersections, Interchanges & Crossings 2020.
- LGAT Tasmanian Standard Drawings



1.5 Statement of Experience and Qualifications

This TIA has been prepared by Richard Burk, an experienced and qualified traffic engineer in accordance with the requirements of the Department of State Growth's guidelines and Council's requirements. Richard's experience and qualifications include:

- 39 years professional experience in road and traffic engineering industry
 - Manager Traffic Engineering at the Department of State Growth until May 2017.
 - Previous National committee membership with Austroads Traffic Management Working Group and State Road Authorities Pavement Marking Working Group
- Master of Traffic, Monash University, 2004
- Post Graduate Diploma in Management, Deakin University, 1995
- Bachelor of Civil Engineering, University of Tasmania, 1987

A handwritten signature in blue ink, appearing to read 'Richard Burk', is placed over a light blue rectangular background.

Richard Burk

BE (Civil) M Traffic Dip Man. MIE Aust CPEng

Director Traffic and Civil Services Pty Ltd



1.6 Glossary of Terms

AADT	Annual Average Daily Traffic - The total number of vehicles travelling in both directions passing a point in a year divided by the number of days in a year.
Acceleration Lane	An auxiliary lane used to allow vehicles to increase speed without interfering with the main traffic stream. It is often used on the departure side of intersections.
Access	The driveway by which vehicles and/or pedestrians enter and/or leave the property adjacent to a road.
ADT	Average Daily Traffic – The average 24-hour volume being the total number of vehicles travelling in both directions passing a point in a stated period divided by the stated number of days in that period.
Austrroads	The Association of Australian and New Zealand road transport and traffic authorities and includes the Australian Local Government Association.
Delay	The additional travel time experienced by a vehicle or pedestrian with reference to a base travel time (e.g. the free flow travel time).
DSG	Department of State Growth – The Tasmanian Government Department which manages the State Road Network.
GFA	Gross Floor Area
Intersection Kerb	The place at which two or more roads meet or cross. A raised border of rigid material formed at the edge of a carriageway, pavement or bridge.
km/h	Kilometres per hour
Level of Service	An index of the operational performance of traffic on a given traffic lane, carriageway or road when accommodating various traffic volumes under different combinations of operating conditions. It is usually defined in terms of the convenience of travel and safety performance.
m	Metres
Median	A strip of road, not normally intended for use by traffic, which separates carriageways for traffic in opposite directions. Usually formed by painted lines, kerbed and paved areas grassed areas, etc.
Movement	A stream of vehicles that enters from the same approach and departs from the same exit (i.e. with the same origin and destination).
Phase	The part of a signal cycle during which one or more movements receive right-of-way subject to resolution of any vehicle or pedestrian conflicts by priority rules. A phase is identified by at least one movement gaining right-of-way at the start of it and at least one movement losing right-of-way at the end of it.



Sight Distance	The distance, measured along the road over which visibility occurs between a driver and an object or between two drivers at specific heights above the carriageway in their lane of travel.
Signal Phasing	Sequential arrangement of separately controlled groups of vehicle and pedestrian movements within a signal cycle to allow all vehicle and pedestrian movements to proceed.
SISD	Safe Intersection Sight Distance – The sight distance provides sufficient distance for a driver of a vehicle on the major road to observe a vehicle on a minor road approach moving into a collision situation and to decelerate to a stop before reaching the collision point.
Speed	Distance travelled per unit time.
85th Percentile	The speed at which 85% of car drivers will travel slower and 15% will travel faster. A control method that allows a variable sequence and variable duration of signal displays depending on vehicle and pedestrian traffic demands.
Traffic-actuated Control	A control method that allows a variable sequence and variable duration of signal displays depending on vehicle and pedestrian traffic demands.
Traffic Growth Factor	A factor used to estimate the percentage annual increase in traffic volume.
Trip	A one-way vehicular movement from one point to another excluding the return journey. Therefore, a vehicle entering and leaving a land use is counted as two trips. (RTA Guide to Traffic generating Developments).
Turning Movement	The number of vehicles observed to make a particular turning movement (left or right turn, or through movement) at an intersection over a specified period.
Turning Movement Count	A traffic count at an intersection during which all turning movements are recorded.
Vehicle Actuated Traffic Signals	Traffic signals in which the phasing varies in accordance with the detected presence of vehicles on the signal approaches.
vpd	vehicles per day – The number of vehicles travelling in both directions passing a point during a day from midnight to midnight.
vph	vehicles per hour – The number of vehicles travelling in both directions passing a point during an hour.

1.7 Site Specific Glossary of Terms

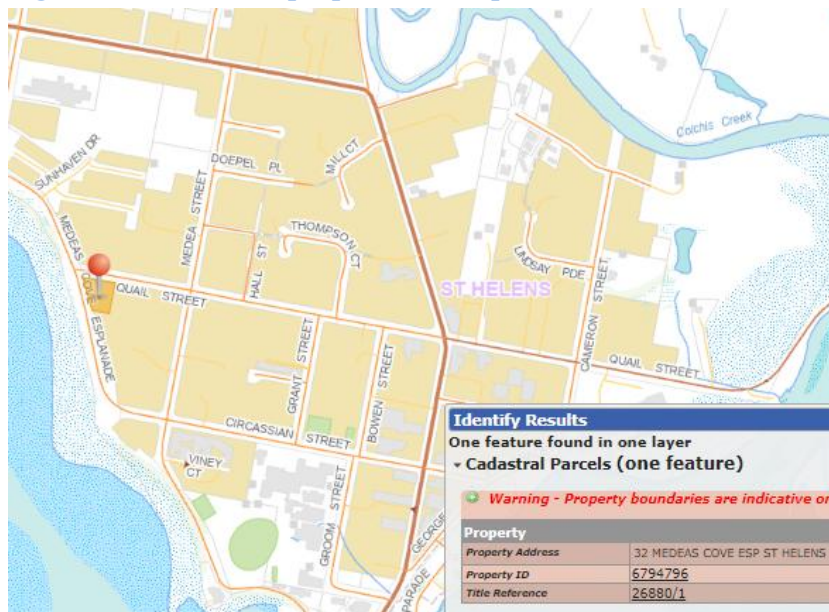
BODC	Break O Day Council
SSA	Safe System Assessment



2. Site Description

The proposed multiple dwelling development site is at the Western End of Quail Street, St Helens on the South side of the road. The development location, road network and proposed layout are shown in Figures 1 to 3.

Figure 1 - Location of proposed development



Source: LISTmap, DPIPWE

Figure 2 – Aerial view of 32 Meade Cove Esplanade with access via Quail Street



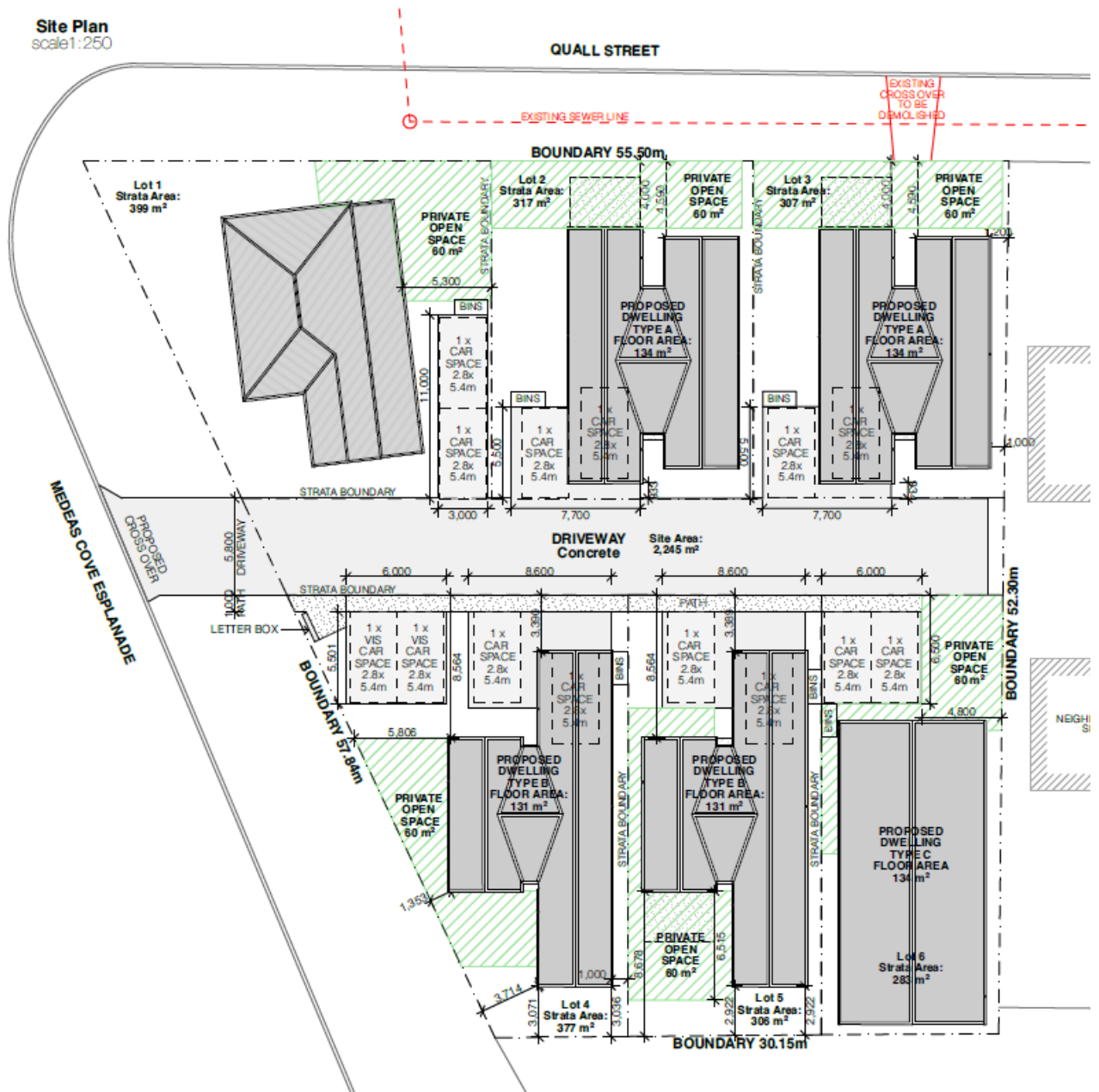
Source: LISTmap, DPIPWE

3. Development Proposal & Planning Scheme

3.1 Description of Proposed Development

6*3bedroom dwelling including the existing house are proposed at 32 Medeas Cove Esplanade, St Helens, see layout shown in Figure 3. A full set of design plans is attached in Appendix D.

Figure 3 – Proposed unit layout

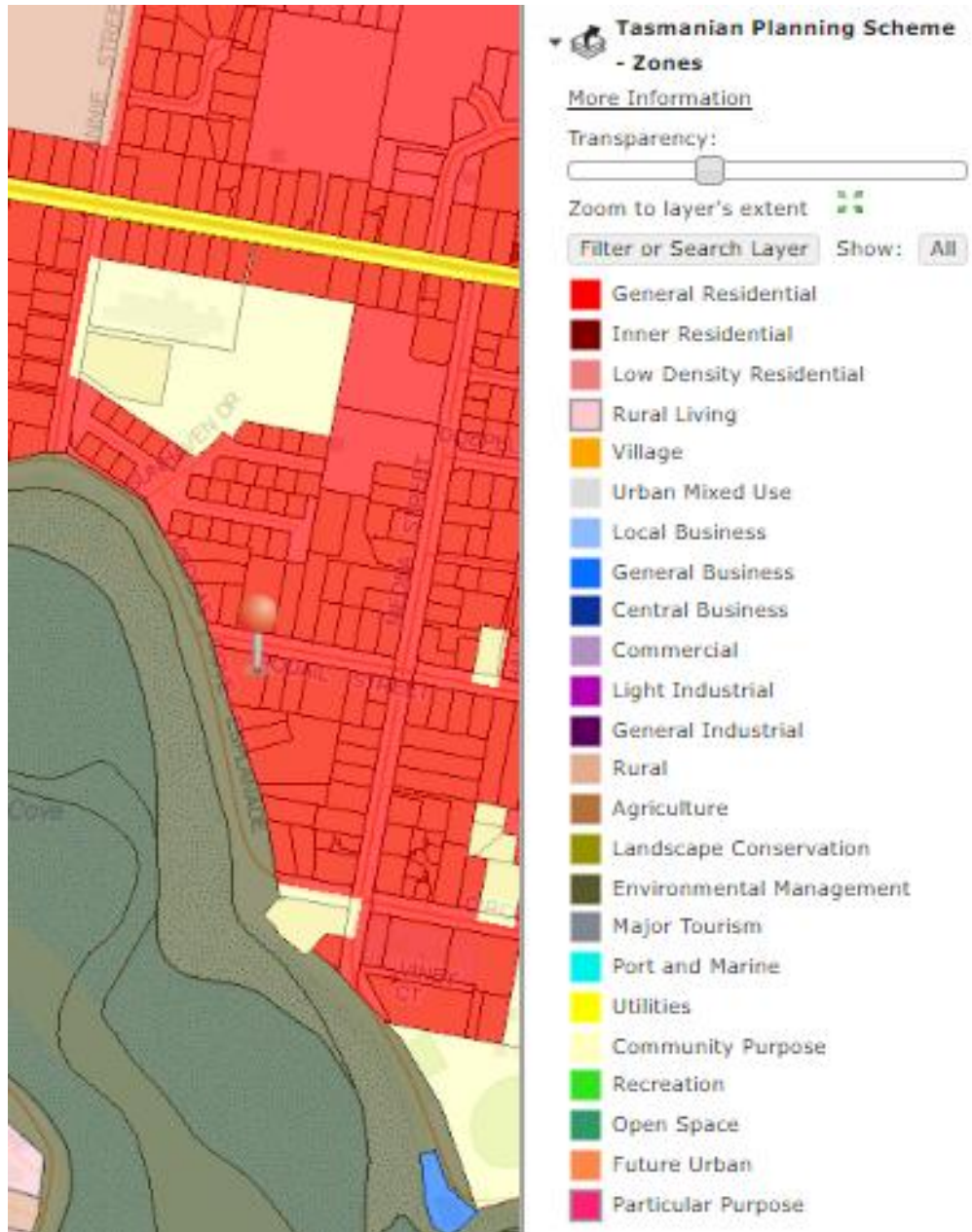




3.2 Council Planning Scheme

The proposed development involves land currently zoned General Residential under the Tasmanian Planning Scheme – Break O Day, see Figure 4.

Figure 4 – The development site is zoned General Residential



Source: LISTmap, DPIPWE

3.3 Council Road Network Objectives

BODC objectives for the Council Road Network are to ensure safe and efficient roads are operated and maintained, and developed in an orderly manner, minimising conflicts between sensitive uses and major roads.

4. Existing Conditions

4.1 Transport Network

The transport network adjacent the proposal consists of Medeas Cove Esp. & Quail St. which connect the area with Cecilia St. which is the Main Rd. through St Helens. The road network at St Helens is not part of the Tasmania's 26m B Double network, see Appendix C. The General Urban Speed Limit of 50 km/h applies.

4.2 Medeas Cove Esplanade

Medeas Cove Esplanade has a Collector function in the Council Road Hierarchy with a sealed width of some 7m on the approaches to the Quail Street junction. Medeas Cove Esplanade does not support on street parking.

North of the Quail St junction there is footpath on the East side of the road. South of the Quail St. junction there are no footpaths however wide, mowed and pedestrian friendly verges are maintained both sides of the road. The street is delineated with streetlighting and guideposts.

4.3 Quail Street

Quail Street functions as a residential street in the Council Road Hierarchy with a sealed width of some 11m. The street has on street parking available both sides with a footpath along the Northern side. The street is delineated with streetlighting.

4.4 Medeas Cove / Quail Street junction

The junction layout and approaches are shown in Figures 5 to 10.

Figure 5 – Aerial view of Medeas Cove / Quail Street junction



Source: LISTmap, DPIPWE



Figure 6 – Looking right along Medeas Cove Esplanade from Quail Street



**Sight distance
right is 140m.**

Figure 7 – Looking left along Medeas Cove Esplanade from Quail Street



Sight distance left is 140m.
**Double headed arrow
indicates proposed access
position**

Figure 8 – Medeas Cove Esplanade Northern approach to junction





Figure 9 – Medeas Cove Esplanade Southern approach to junction



Figure 10 – Quail Street approach to Medeas Cove Esplanade



4.5 Proposed 32 Medeas Cove Esplanade access

The existing access to 32 Medeas Cove Esplanade is on Quail Street some 50m East of Medeas Cove Esplanade. Figure 11 shows the relocated access position with a double headed arrow. Figures 11 to 16 show the access approaches. See Section 6.7 for discussion of the recommended access standard.

Figure 11 – Aerial view of proposed access to 32 Medeas Cove Esp.



Source: LISTmap, DPIPWE



Figure 12 – Looking left along Medeas Cove Esp. from proposed access to #32



The natural surface of the verge area drains to the stormwater culvert pictured.

Figure 13 – Looking right along Medeas Cove Esp. from proposed access



Sight distance right is 130m.

Figure 14 – Looking left along Medeas Cove Esp. from proposed access



Sight distance left is 130m.



Figure 15 – Medeas Cove Esp. Southern approach to proposed access



Figure 16 – Medeas Cove Esp. Northern approach to proposed access



4.6 Sight Distance Summary (Figure 17)

Figure 17 – Sight distance requirements summary

Junction Major Rd - Minor Rd	Speed Limit (km/h)	Speed Environment (km/h)	Road frontage sight distance			
			Austroads SISD (m)	Available		AS 2890.1 SSD(m)
				Left(m)	Right(m)	
Medeas Cove - Quail	50	50	97	140	140	
32 Medeas Cove	50	50	97	130	130	45

SISD compliant



4.7 Traffic Activity (AADT)

Quail Street

- Western side of Cecilia St: 1,950 vpd (2019 from BODC Data)
- Western end of Quail St: 300 vpd (2026 estimated by TCS)

Medeas Cove Esplanade

- Southern end: 1,000 vpd (2026 estimated by TCS).
- At Quail St: 300 vpd (2026 estimated by TCS).

4.8 Crash History

The DSG is supplied with reported crashes by Tasmania Police. The DSG maintains a crash database from the crash reports which is used to monitor road safety, identify problem areas and develop improvement schemes.

The 5-year crash history for the Medeas Cove Esplanade / Quail Street junction and approaches records 1 PDO type crash though the crash description is not provided and the crash site appears to be off street.

The overall crash history is summarised in Figures 18 & 19. There is no evidence of a crash propensity in the vicinity of the access to the 32 Medeas Cove Esplanade property or at the adjacent junction.

Figure 18 – Medeas Cove Esp. / Quail St. junction 5 Year Reported Crash History

Crash Id	Units	Description	Crash Fact	Date	Time	Severity	Light	Location
52783787	LV	-	Undetermined	20-JUN-2025	09:00	PDO	Day	St Helens

LV | Light vehicle

PDO | Property Damage Only

Figure 19 – Medeas Cove Esp. / Quail St. junction crash locations





4.9 Medeas Cove Esp. / Quail St. & approaches Road Safety Review

From site inspection and road safety review, no roadside hazards or traffic safety issues with the approaches to the proposed 32 Medeas Cove Esplanade property access or at the nearby Medeas Cove Esplanade / Quail Street junction were identified.

4.10 Safe Systems Assessment

Medeas Cove Esplanade has been assessed in accordance with the Austroads Safe System Assessment framework.

This framework involves consideration of exposure, likelihood and severity to yield a risk framework score. High risk crash types and vulnerable road user crash types are assessed for each site and aggregated to provide an overall crash risk. Crash risk is considered in terms of three components:

- Exposure (is low where low numbers of through and turning traffic) i.e. 1 out of 4
- Likelihood (is low where the infrastructure standard is high) i.e. 1 out of 4
- Severity (is low where the speed environment is low) i.e. 1 out of 4

The Austroads Safe System Assessment process enables the relative crash risk of an intersection or road link to be assessed. Vulnerable road users are considered along with the most common crash types.

Crash risk scores indicate how well the infrastructure satisfies the *safe system objective which is for a forgiving road system where crashes do not result in death or serious injury*.

The SSA crash risk was assessed as 22/448 for Medeas Cove Esplanade which is a low crash risk score. See Appendix B for the assessment details. Figure 20 indicates the severity of the SSA score.

Figure 20 – Austroads Safe System Assessment alignment between crash score and risk





5. Traffic Generation and Assignment

This section of the report describes how traffic generated by the proposal is distributed within the adjacent road network and in ten years (2036).

5.1 Traffic Growth

Medeas Cove Esplanade

Background traffic growth of 0.5% compound annual growth rate is assumed allowing for ongoing development within St Helens.

- AADT 300 vpd. (2026)
- AADT 315 vpd. (2036)

5.2 Trip Generation

For Medium Density Residential Dwellings, NSW Guide to Transport Impact Assessment 2024 – Section 5.6.2 Residential traffic generation guidelines are:

- 3.67 vehicle movements per day
- 0.41 vehicle movements during AM peak hour
- 0.60 vehicle movements during PM peak hour

For a 6 Lot multiple dwelling development as proposed estimated traffic generation is:

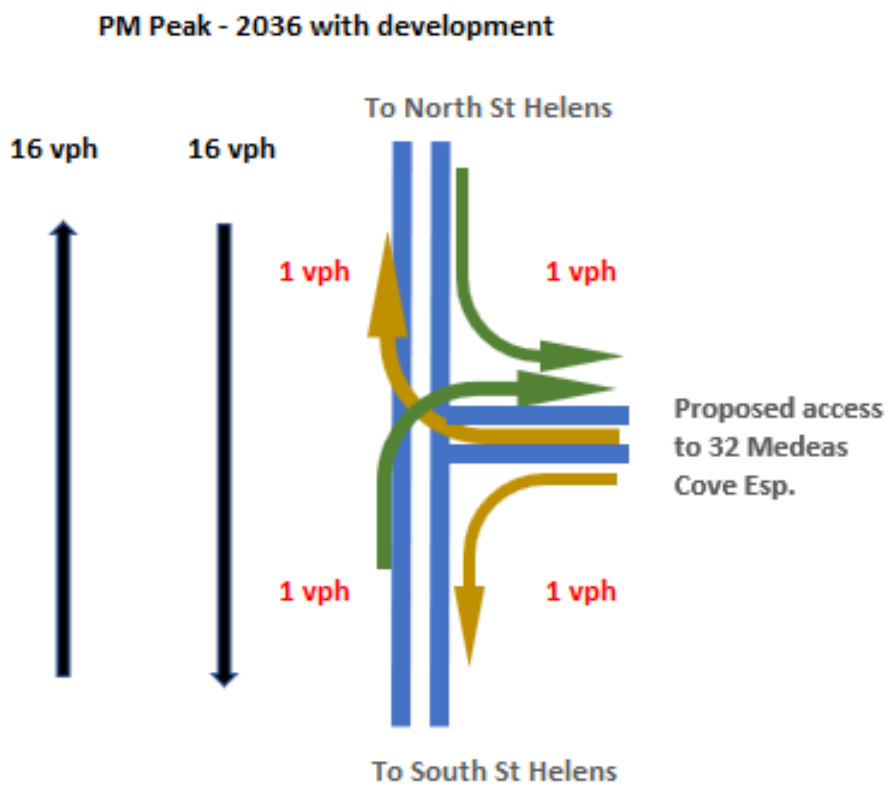
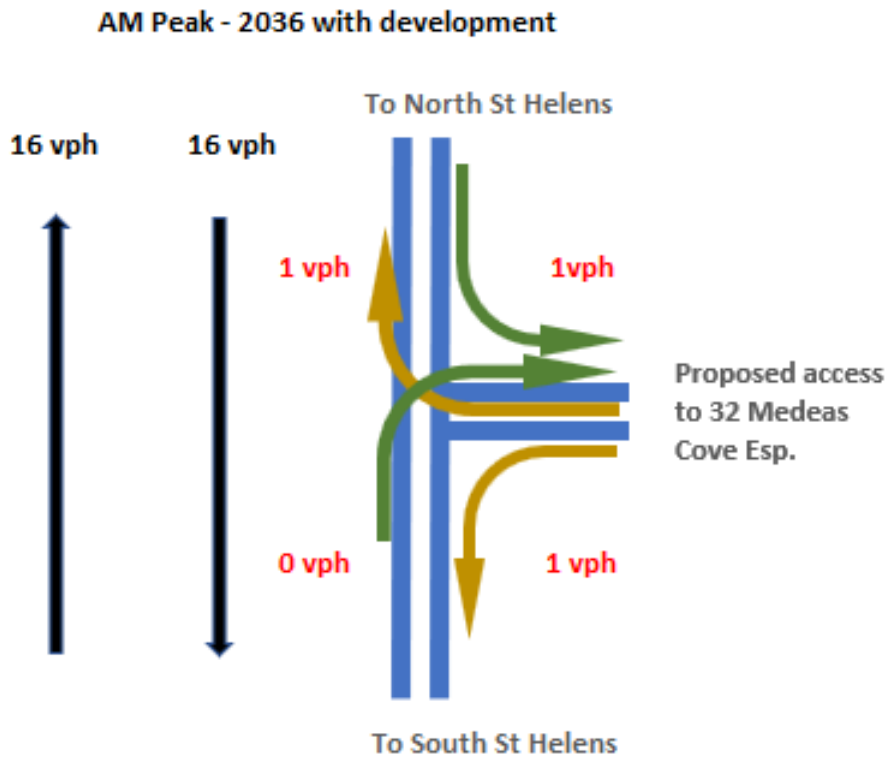
- 22 vpd
- 3 vph during AM peak hour
- 4 vph during PM peak hour

5.3 Trip Assignment

Trip assignment at the proposed access to 32 Medeas Cove Esp. is shown in Figure 21.



Figure 21 - Assigned traffic at proposed access to 32 Medeas Cove Esplanade.



6. Impact on Road Network

6.1 Impact of traffic generated by the proposal

This section considers the performance of the proposed access in 2036 based on assumed background traffic growth of 0.5% and the traffic generated by the proposal.

6.2 32 Medeas Cove Property access

The proposal relocates the property access from Quail Street to Medeas Cove Esplanade. The existing access crossover will be removed and reinstated with kerb and channel and nature strip. The proposal is estimated to increase traffic on Medeas Cove Esplanade by 22 vpd & 3 to 4 vph by 2036. At these traffic levels the access will continue to operate at LOS A by 2036. See LOS descriptions in Appendix A.

6.3 Medeas Cove Esplanade / Quail Street Junction

The proposal is estimated to increase traffic on Quail Street by 22 vpd & 4 vph by 2036 and have negligible impact on the junction operation which will continue to operate at LOS A.

6.4 Austroads Junction Warrant

The junction layout required is based on Austroads Guidelines which take into account the standard of the road, speed limit and volume of through and side road traffic. See Figure 22 for Austroads junction warrant. Technically the proposal warrants an Austroads BAR junction.

Figure 22 – Austroads Junction Warrants for Medeas Cove Esp. / Quail St. junction

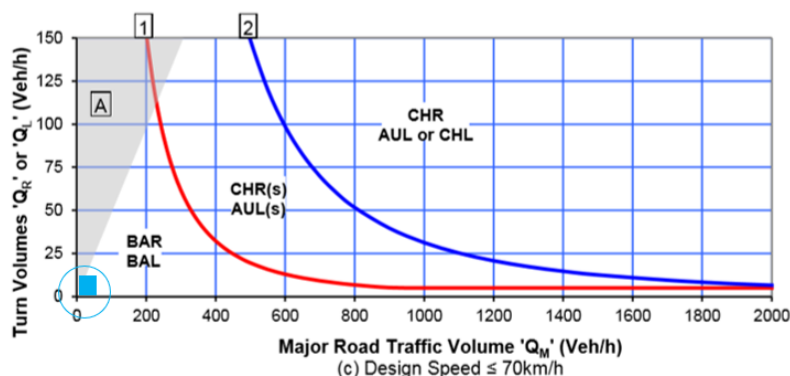


Figure 22 provides evidence that by 2036:

- A Simple Left turn facility would be adequate for left turns entering the access.
- A BAR right turn facility is technically required for right turns entering the access.

However, as the turning traffic (some 10 vph) and through traffic of 30 vph are very low, the existing simple junction is adequate.



6.5 Tasmanian Planning Scheme – Break O Day

6.5.1 Parking and Sustainable Transport Code C2

C2.5.1 Car parking numbers

Acceptable Solution A1

The number of on-site car parking spaces must be no less than the number specified in Table C2.1, excluding if:

- (a) The site is subject to a parking plan for the area adopted by Council, in which case parking provision (spaces or cash in lieu) must be in accordance with that plan,*
- (b) The site is contained within a parking precinct plan and subject to Clause C2.7,*
- (c) The site is subject to Clause C2.5.5; or*
- (d) It relates to an intensification of an existing use or development or a change of use where:*
 - i. The number of onsite car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional onsite car parking is required; or*
 - ii. The number of onsite car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:*

$$N=A+(C-B)$$

N = Number of on-site car parking spaces required

A = Number of existing on-site car parking spaces

B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1.

C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1.

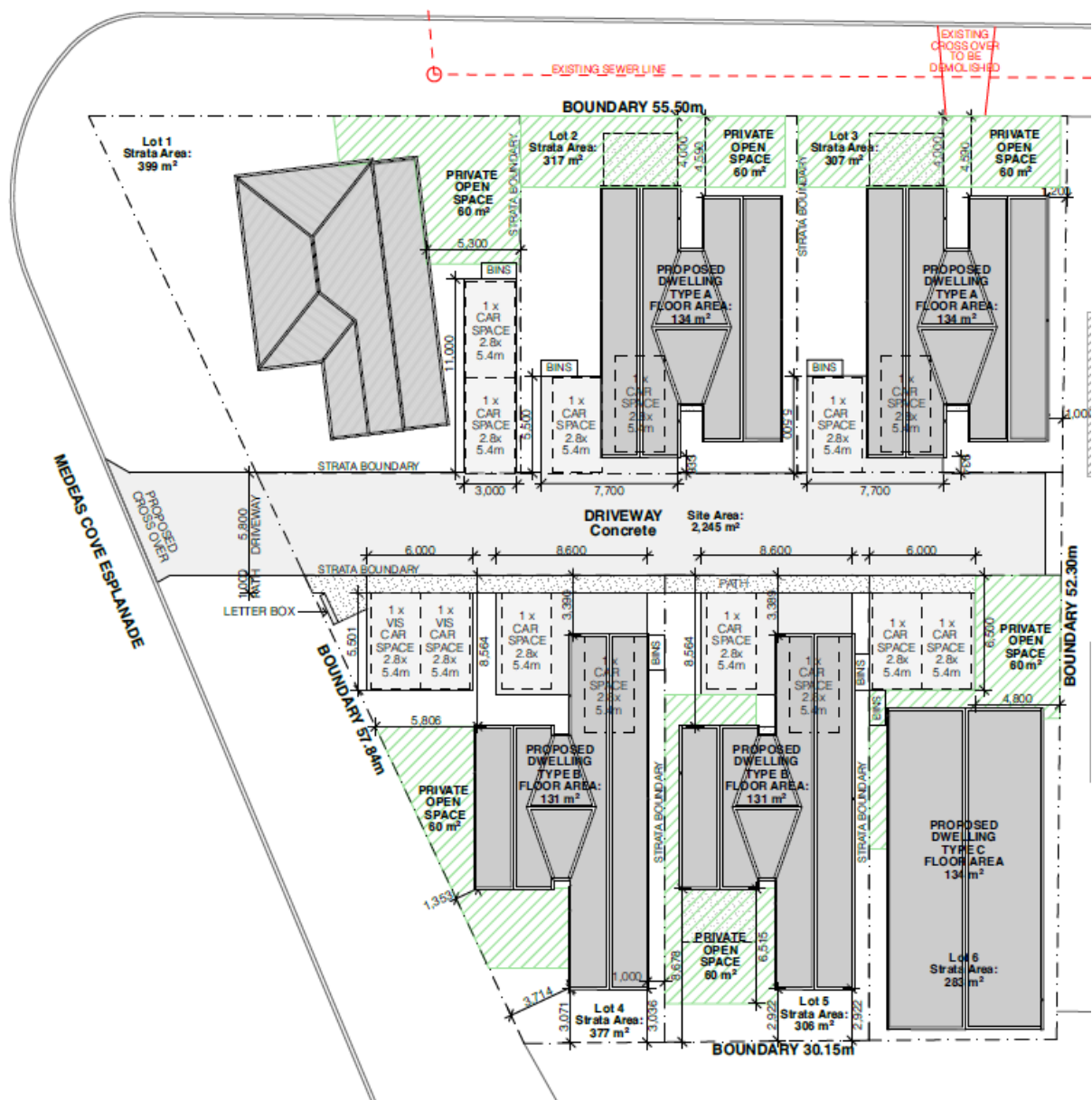
Table C2.1 parking requirements for 2 or more-bedroom units are as follows:

- Resident parking – 2 spaces per unit, for 6 units 12 spaces are required.
- Visitor parking – 1 space per 3 units, for 6 units 2 spaces are required.

The proposal provides 14 resident and visitor parking spaces, see Figure 23.

A1 is satisfied.

Figure 23 – Proposed off street parking at 32 Medeas Cove Esplanade



C2.5.2 Bicycle parking numbers

No requirement.

C2.5.3 Motorcycle parking numbers

No requirement where the number of car parking spaces required is 0-20.

C2.5.4 Loading Bays

Acceptable Solution A1: A loading bay must be provided for uses with a floor area of more than 1,000m² in a single occupancy. - **A1 is not applicable.**



C2.6.1 Construction of parking areas

Acceptable Solution A1: All parking, access ways, manoeuvring and circulation spaces must:

- (a) be constructed with a durable all-weather pavement,
- (b) be drained to the public stormwater system, or contain stormwater on the site; and
- (c) excluding all uses in the Rural Zone, Agricultural Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Public Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.

Sealed & drained parking spaces & driveway are proposed, see Appendix D. **A1 is satisfied.**

C2.6.2 Design and layout of parking areas

Acceptable Solution A1.1: Parking, accessways, manoeuvring & circulation spaces must either:

- (a) comply with the following:
 - i. have a gradient in accordance with Australian Standard AS 2890 Parking facilities, Parts 1-6. The site is flat with maximum grade in the order of 3%.
 - ii. Provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces. Satisfied
 - iii. Have an access width not less than the requirements in Table C2.2.
The proposed driveway will be 5.8m wide satisfying the minimum 4.5m for the first 7m from the crossover as per Table C2.2 and otherwise constructed in accordance with LGAT Standard Drawing TSD – R03- Rural driveway standard & Appendix E.
 - iv. Have car parking space dimensions which satisfy Table C2.3.
90-degree resident & visitor parking spaces meet minimum 2.8m wide by 5.4m long with 5.8m of manoeuvre space requirement. Table C2.3 is satisfied.
 - v. Have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces.
Adequate 5.8m manoeuvre space is provided compliant with Table C2.3.
 - vi. Have a vertical clearance of not less than 2.1 metres above the parking surface level, Satisfied.
 - vii. Excluding a single dwelling, be delineated by line marking or other clear physical means. Satisfied.
- (b) Comply with Australian Standard AS 2890 Parking facilities, Parts 1-6. Satisfied.

A1.1 is satisfied.



Acceptable Solution A1.2

Parking spaces provided for use by persons with a disability must satisfy the following:

- (a) Be located as close as practical to the main entry point to the building. Satisfied.*
- (b) be incorporated into the overall car park design. Satisfied.*
- (c) be designed and constructed in accordance with Australian/ New Zealand Standard AS/NZS 2890.6-2009 Parking facilities - Off-street parking for people with disabilities. - **Not Applicable.***

C2.6.3 Number of accesses for vehicles

Acceptable Solution A1

The number of accesses provided for each frontage must:

- (a) be no more than 1; or*
- (b) no more than the existing number of accesses whichever is greater.*

A relocated existing two-way access is proposed. **A1 is satisfied.**

C2.6.5 Pedestrian access

Acceptable Solution A1.1

Applies to uses that require 10 or more car parking space must:

- (a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:
 - i. a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or*
 - ii. protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and**
- (b) be signed & line marked at points where pedestrians cross access ways or parking aisles.*

The proposal requires 14 car parking spaces and proposes footpath closer to the access way than 2.5m due to site constraints i.e lack of width. **A1.1 is not satisfied.**

Performance Criteria P1

Safe and convenient pedestrian access must be provided within parking areas, regarding:

- (a) the characteristics of the site.*
- (b) the nature of the use*
- (c) the number of parking spaces*
- (d) the frequency of vehicle movements*
- (e) the needs of persons with a disability*
- (f) the location and number of footpath crossings*
- (g) vehicle and pedestrian traffic safety*
- (h) the location of any access ways or parking aisles*
- (i) any protective devices proposed for pedestrian safety.*



The site has insufficient width to provide footpath and 2.5m separation to the driveway.

The use is residential in a low-speed environment < 30km/h.

Adequate off-street parking is proposed.

Vehicle activity is low with peak vehicle movements at some 4 vph.

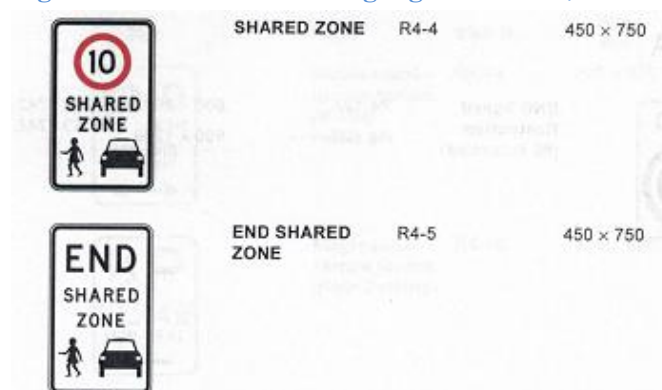
Sealed surfaces are provided in all areas conducive to safe pedestrian use.

From Austroads Safe System Assessment crash risk is considered very low:

- Crash exposure is very low as traffic activity levels are low and pedestrian activity in the vicinity of vehicles is low i.e low crash exposure.
- Crash likelihood is low as Australian Standard off street parking is provided, sight lines are open and pedestrian activity and parking areas are reasonable separate and at low activity levels.
- Crash severity is low as the vehicle speed environment is low < 30km/h.

Formal signage of shared zones is a recognised pedestrian safety improvement where there is a mix of pedestrian, local access traffic only and situation where this is no kerb separation between pedestrians and vehicles. This is because Shared Zone signage includes provision of a regulator speed limit to keep speed to an appropriate level. In the case of the proposed driveway a 10 km/hr speed limit is considered normal. The proposed development is in keeping with this kind of situation. Figure 24 shows Shared Zone signage standards.

Figure 24 – Shared Zone signage standards, AS1742.1-2021



Accordingly, TCS recommends acceptance of the proposal with provision of 10km/hr Shared and End Shared Zone signage at the entry and exit to the development to limit speeds to a safe level. **P1 is satisfied.**

Acceptable Solution A1.2

*In parking areas containing accessible car parking spaces for uses by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building. **1.2 is not applicable.***



6.5.2 Road and Railway Assets Code C3

C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction

Acceptable Solution A1.1 – For a category 1 road or a limited access road, vehicular traffic to and from the site will not require:

- (a) A new junction*
- (b) A new vehicle crossing*
- (c) A new level crossing*

Not applicable as the roads are not Category 1.

Acceptable Solution A1.2 – For a road, excluding a Category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority.

A1.2 is not satisfied as no written consent has been issued by the road authority for the proposed access widening. This TIA is to assist Council decision making.

Acceptable Solution A1.3 – For the rail network, written consent for a new private level crossing to serve the use and development has been issued by the rail authority.

Not applicable as no rail network is involved.

Acceptable solution A1.4:

Vehicular traffic to and from the site, using and existing vehicle crossing or private level crossing will not increase by more than:

- (a) The amounts in Table C3.1*
- (b) Allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road; and*

The proposal is estimated to generate 22 vpd on Medeas Cove Esplanade which is classed *other road*. From Table C3.1 for vehicle crossings on *other roads*, the acceptable increase in AADT at the site is 20% or 40vpd whichever is greater. **A1.4 is satisfied.**

A1.5: Vehicular traffic must be able to enter and leave a major road in a forward direction.
A1.5 is satisfied.

C3.6.1 Habitable buildings for sensitive uses within a road or railway attenuation area

Not applicable as the proposal does not involve a road or railway attenuation area.

C3.7.1 Subdivision for sensitive uses within a road or railway attenuation area

Not applicable as the proposal does not involve a road or railway attenuation area.



6.6 Other requirements

6.6.1 Environmental

No adverse environmental impact is anticipated in relation to:

- Noise, Vibration and Visual Impact
- Community Severance and Pedestrian Amenity
- Hazardous Loads, Air Pollution and Dust and Dirt
- Ecological Impacts and Heritage and Conservation

6.6.2 Street Lighting and Furniture

No Street lighting is required.

6.7 Property access standard

In the case of Medeas Cove Esplanade the access standard applicable needs to be consistent with Council policy. The following Figures 25 – 27 show the typical property access standard along Medeas Cove Esplanade.

Figure 25 – Looking North along Medeas Cove Esp. from Fishermen’s Memorial Park



Rural property access standard with sealed driveway at grade with the road.



Figure 26 – Looking North along Medeas Cove Esp. from Quail St



Rural property access standard with sealed driveway at grade with the road.

Figure 27 – Looking North along Medeas Cove Esp. from Heather Place



Rural property access standard with sealed driveway at grade with the road.

Accordingly for the proposed access to 32 Medeas Cove Esplanade a similar standard is proposed consistent with the LGAT Standard Drawing for Rural Roads – Typical Property Access TSD-R03, see Appendix E.

From site inspection a stormwater culvert is not necessary as the verge drains toward the culvert on Medeas Cove Esplanade well left of the proposed access position, see Figure 12. The verge to the right of the proposed access drains to the existing kerb and channel leading to Quail Street, see Figure 7.



6.8 Tasmanian Subdivision Guideline Considerations

No issues have been identified.

6.9 Transport Planning Considerations

No issues have been identified.

6.10 Provisions for all road users

6.10.1 Light Vehicles

Traffic safety and capacity requirements for light vehicles have been considered and are considered suitable in terms of traffic safety and capacity.

6.10.2 Garbage Collection

Kerbside wheelie bin processing is assumed.

6.10.3 Public Transport

Medeas Cove Esplanade is a school bus route with stops at regular intervals in the vicinity of St Helens District High School.

6.10.4 Vulnerable Road Users

Pedestrians

The proposal does not affect pedestrians.

Cyclists

The proposal does not affect cyclists.

Motorcyclists

The proposal does not affect motorcyclists.

6.11 Services

The proposal does not appear to affect above or below ground services.



7. Recommendations and Conclusions

This traffic impact assessment has been prepared to assess the proposed 6-unit multiple dwelling development at 32 Medeas Cove Esplanade, St Helens and compliance with the Tasmanian Planning Scheme – Break O Day, Parking & Sustainable Transport Code C2 and Road & Railway Assets Code C3 requirements.

It has been prepared following a review of traffic and crash data, existing conditions, Road Safety Review, Austroads Safe System Assessment, future growth projections for Medeas Cove Esplanade traffic, relevant traffic standards and Austroads guidelines.

From Road Safety Review and Safe System Assessment Medeas Cove Esplanade in the vicinity of the site access and nearby junction with Medeas Cove Esplanade is determined to have a very low crash risk.

The 5-year reported crash history on the approaches to the Medeas Cove Esplanade and Quail Street junction provides no evidence of a crash propensity in the vicinity of the proposed access.

The proposal is estimated to generate 22vpd and 4 vph at peak times. No changes to the Medeas Cove Esplanade / Quail Street junction are required due to the proposal.

This report provides evidence that the proposal satisfies the Tasmanian Planning Scheme – Break O Day, Parking & Sustainable Parking Code C2 and Road & Railway Assets Code C3 requirements.

Recommendations

- *Construct proposed driveway to proposed width and otherwise consistent with LGAT Standard Drawing TSD-R03, see Appendix E, except that a driveway culvert is not required as there are existing stormwater drainage provisions, see Section 6.7.*
- *Install 10km/h Shared Zoned signage for traffic entering the driveway & End Shared Zone signage for traffic exiting the driveway onto Quail Street, see Figure 24.*

Overall, it has been concluded that based on the findings of this report and subject to the recommendations described above, the proposed multiple dwelling development will operate safely and efficiently and not disrupt operation of Medeas Cove Esplanade or the junction with Quail Street. On this basis the proposal is supported on traffic grounds.



Appendices



Appendix A - Level of Service Descriptions

Level of service A	A condition of free-flow in which individual drivers are virtually unaffected by the presence of others in the traffic stream. Freedom to select desired speeds and to manoeuvre within the traffic stream is extremely high, and the general level of comfort and convenience provided is excellent.
Level of service B	In the zone of stable flow where drivers still have reasonable freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience is a little less than with level of service A.
Level of service C	Also in the zone of stable flow, but most drivers are restricted to some extent in their freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience declines noticeably at this level.
Level of service D	Close to the limit of stable flow and approaching unstable flow. All drivers are severely restricted in their freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience is poor, and small increases in traffic flow will generally cause operational problems.
Level of service E	Traffic volumes are at or close to capacity, and there is virtually no freedom to select desired speeds or to manoeuvre within the traffic stream. Flow is unstable and minor disturbances within the traffic stream will cause breakdown.
Level of service F	In the zone of forced flow, where the amount of traffic approaching the point under consideration exceeds that which can pass it. Flow breakdown occurs, and queuing and delays result.



Appendix B - Safe Systems Assessment

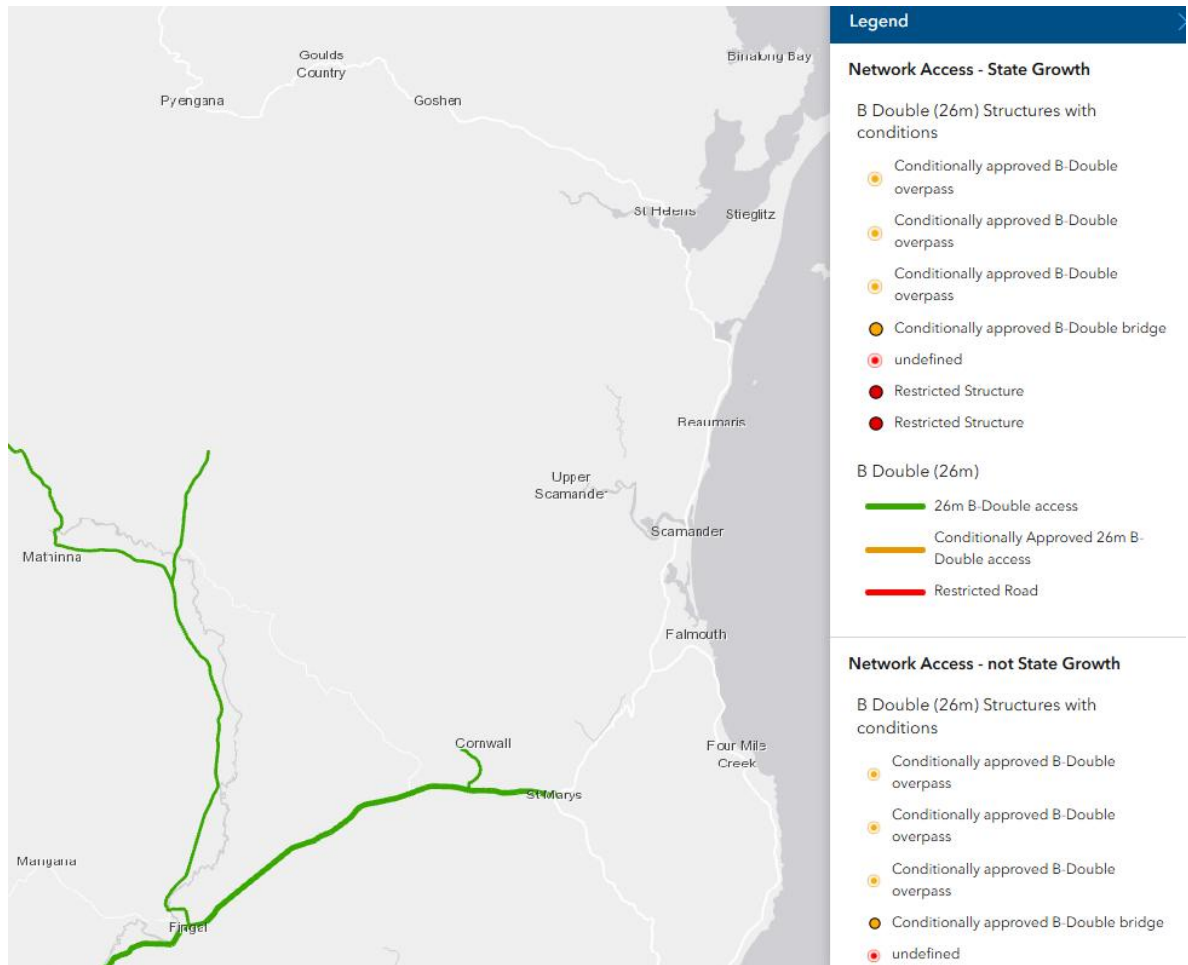
Medeas Cove Esplanade approaches to proposed access to #32

Safe System Assessment

Exposure	Run-off-road	Head-on	Intersection	#32 Access	Pedestrian	Cyclist	Motorcyclist
Justification (AADT 300vpd)	Low traffic, no run off road crashes	Low traffic, no head on crashes	Low traffic i.e 300vpd on all legs, no reported crashes.	Low Traffic, no crashes	Low traffic and low to moderate pedestrian activity, no pedestrian crashes.	Low traffic and low cyclist activity, no cyclist crashes	High traffic and motorcyclist activity, one minor injury motorcycle crash
Likelihood	Score / 4	Score / 4	Score / 4	Score / 4	Score / 4	Score / 4	Score / 4
	1	1	1	1	1	1	1
Justification	3.5m traffic lanes, straight alignment and delineation with few roadside hazards and adequate sight distance	3.5m traffic lanes, straight alignment and delineation with few roadside hazards and adequate sight distance	Simple junction layout	3.5m traffic lanes, straight alignment and delineation with few roadside hazards and adequate sight distance	No footpaths but wide, mowed and well maintained pedestrian friendly verge both sides of the road.	3.5m traffic lanes, straight alignment and delineation with few roadside hazards and adequate sight distance	3.5m traffic lanes, straight alignment and delineation with few roadside hazards and adequate sight distance
Severity	Score / 4	Score / 4	Score / 4	Score / 4	Score / 4	Score / 4	Score / 4
	2	2	2	1	2	1	2
Justification (50km/h speed limit)	Low speed environment	Low speed environment	Low speed environment	Low speed environment	Moderate to high speed environment for pedestrians	Moderate to high speed environment for cyclists	Moderate to high speed environment for motorcyclists
Product	Score / 4	Score / 4	Score / 4	Score / 4	Score / 4	Score / 4	Score / 4
	1	1	1	1	3	3	3
Total	Score / 64	Score / 64	Score / 64	Score / 64	Score / 64	Score / 64	Score / 64
	2	2	2	1	6	3	6
							Total / 448
							22



Appendix C - Tas. 26m B Double Network





Appendix D - Development Design Plans

SITE INFORMATION
 Title Reference Number: 26880/1
 Wind Classification: TBC
 Soil Classification: TBC
 Climate Zone: 7
 BAL Level: N/A
 Alpine Area: TBC
 Corrosion Environment: Moderate

Volume / Folio
 Refer to Site Assessment Report
 Refer to Site Assessment Report
 TBC
 NA / Refer to Site Assessment Report
 Refer to NCC

For steel subject to the influence of salt water, breaking surf or heavy industrial areas, Medium; NCC Vol. 2 Part 7.2.2 (Sheet roofing) & Part 6.3.9 (Structural Steel). Cladding and fixings to manufacturer's recommendations.

High wind, earthquake, flooding, landslip, dispersive soils, sand dunes, mine subsidence, landfill, snow & ice or other relevant factors.

NCC/BCA 2022 Note:
 All NCC references are to National Construction Code NCC 2022 Volume 2 & Housing Provisions.

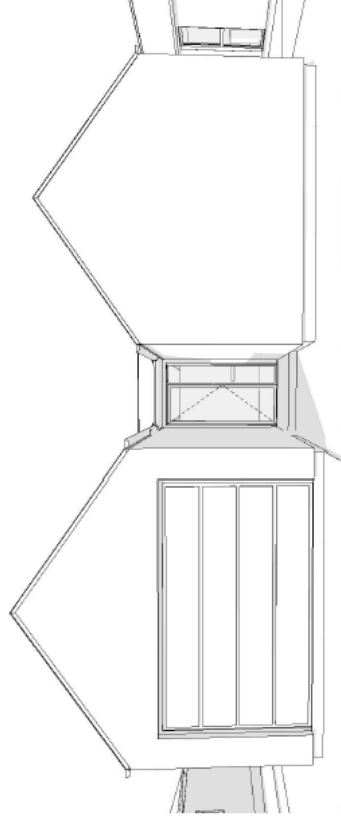
Other Hazards: TBC

SITE AREA

Lot 1	399m ²
Lot 2	317m ²
Lot 3	307m ²
Lot 4	377m ²
Lot 5	306m ²
Lot 6	283m ²
Total	2,245m ²

FLOOR AREA

Lot 1- Existing Dwelling	136m ²
Lot 2- Proposed Dwelling	134m ²
Lot 3- Proposed Dwelling	134m ²
Lot 4- Proposed Dwelling	131m ²
Lot 5- Proposed Dwelling	131m ²
Lot 6- Proposed Dwelling	134m ²
Total	800m ²



DRAWING SCHEDULE

ID	Layout Name
01	Cover
02	Site Plan
03	Floor Plan- Proposed Dwelling- Type A
04	Elevation- Proposed Dwelling- Type A
05	Floor Plan- Proposed Dwelling- Type B
06	Elevation- Proposed Dwelling- Type B
07	Floor Plan- Proposed Dwelling- Type C
08	Elevation- Proposed Dwelling- Type C
09	Floor Plan- Existing Dwelling
10	Elevation- Existing Dwelling





LEGEND
 Stormwater 100mm UPVC
 Ag drain 100mm socketed slotted
 Sewer 100mm UPVC
 Water

GENERAL NOTES
 1. All works must be located onsite prior to excavation.

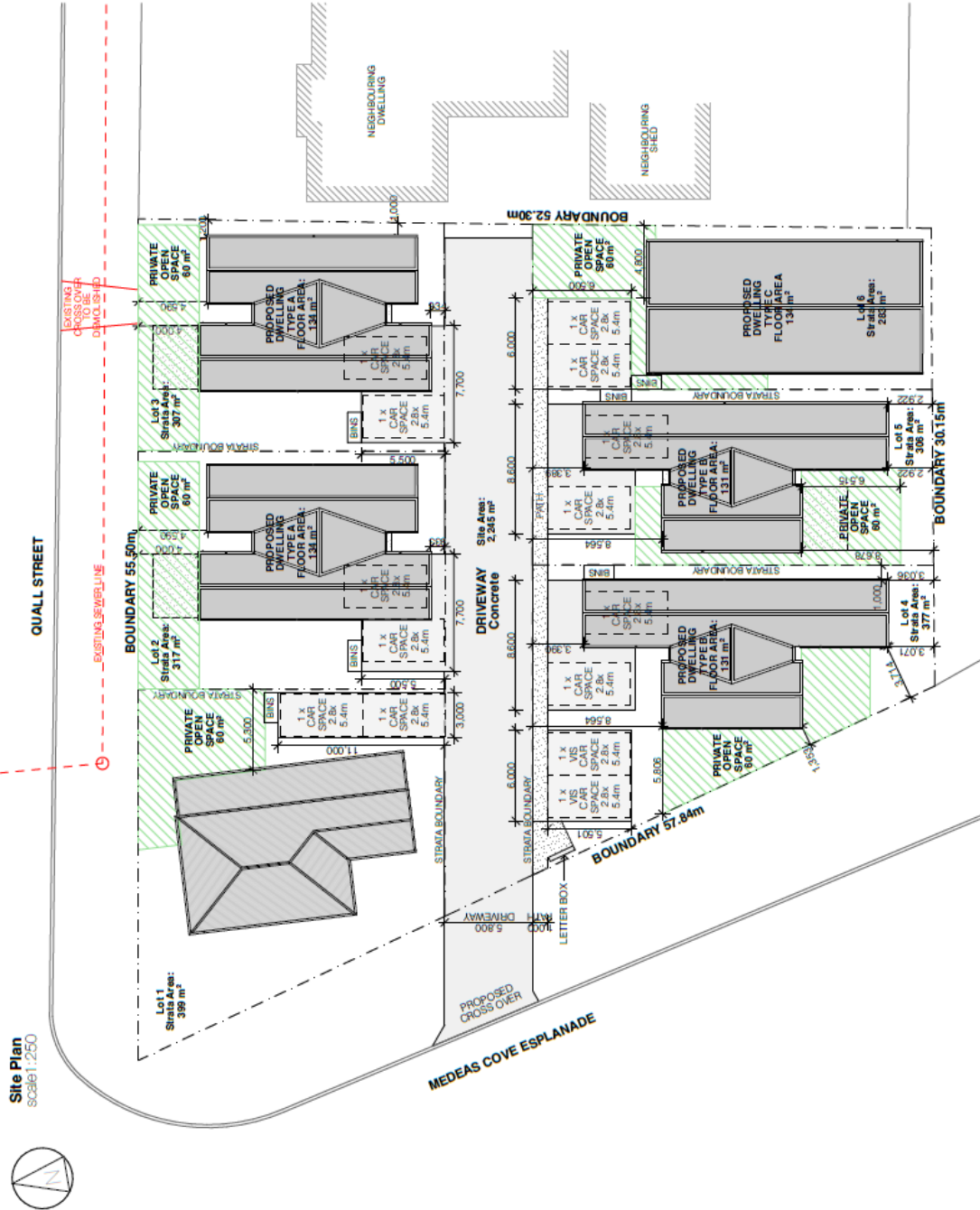
2. Exact locations of existing buildings and boundaries are subject to survey and must be confirmed onsite prior to construction.

3. Bulk excavation must not impede on nearby structures, neighbouring property or boundary fences.
 4. Surface drainage shall be designed and constructed to avoid water ponding against or near the footing in accordance with AS2670. The ground level must be finished to a level above the top of the footing and the ground level must be compacted and graded to a minimum of 50mm minimum away from the footing over a distance of 1m and compacted and graded to ensure drainage of water away from the building.

5. All works must be in accordance with relevant Australian Standards, the National Building Code, and relevant regulatory bodies including local council.

Soil & Water Management Strategies
 6. Stormwater or tanks as soon as practicable.

7. Exact locations of stormwater and wastewater on-site wastewater system to be confirmed onsite.



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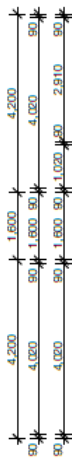
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Revision: _____
 PAGE: _____
 02 of 10



Floor Plan - Proposed Dwelling - Type A
Scale 1:100



- LEGEND**
- Proposed wall
 - Existing levels
 - New levels
 - FL - Finished floor level (top of particle board / slabs)
- Fixtures & Fittings**
- AC - Air Conditioner
 - AC-OUT - Air Conditioner- Outdoor Unit
 - BI - Built-in Cupboard
 - BS - Basin
 - CJ - Control joint
 - CT - Cooktop
 - CL - Curtain
 - DL - Down Light
 - CL - Ceiling Level
 - DP - Down pipe
 - FFL - Finished Floor level
 - FR - Fridge
 - FP - Fire Pit
 - FP - Fire Pit
 - GB - Glass balustrade
 - GD - Garage Door
 - GD - Grated Drain
 - HWS - Hot Water System
 - LDRY - Laundry
 - LD - Linen
 - NBN - NBN Modem
 - NBN-EXT - NBN connection box
 - OV - Oven
 - OV - Oven
 - RWH - Rainwater Head
 - SHR - Shower
 - SK - Skylight
 - S - Sink
 - TAP - Water Tap
 - TR - Trough
 - TV - Television
 - WC - Water Closet
 - WH - Washing Machine
 - WM - Washing Machine
 - WR - Walk-in Robe
 - WP - Walk-in Pantry
- Material/ Finish**
- Conc - Concrete paving slab.
 - Tim - Timber
 - Paving - Paving
 - Screen - Screen
 - Srn - Sarn
 - Tile - Floor tile
 - Tim - Flank flooring
- GENERAL NOTES**
- Finished landscaping must prevent water build up and ensure that the driveway is protected from damage by providing sufficient fall and drainage systems in accordance with Australian Standards.
- All works must be in accordance with relevant Australian Standards, the National Building Code, manufacturer's specification and all other relevant regulatory bodies including local council.
- Refer to Builder's scope of works for joinery inclusions.



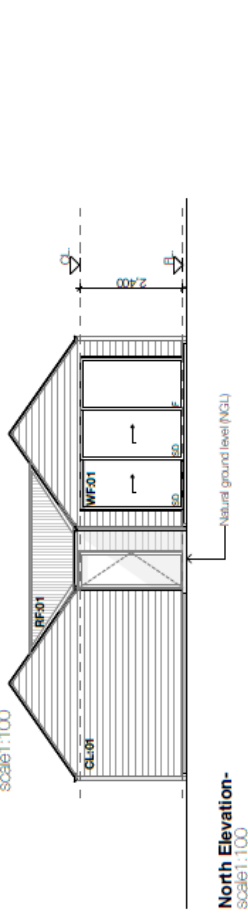
LEGEND

- AW - Awning window
- BB - Ballen balustrade
- CJ - Cantor joint
- CS - Casings
- DF - Downspout
- F - Fixed window
- GB - Glass balustrade
- GD - Garage door
- HD - Glass door
- LD - Landing door level
- SL - Sliding door
- SW - Sliding window
- TAP - Water tap
- WB - Wire balustrade

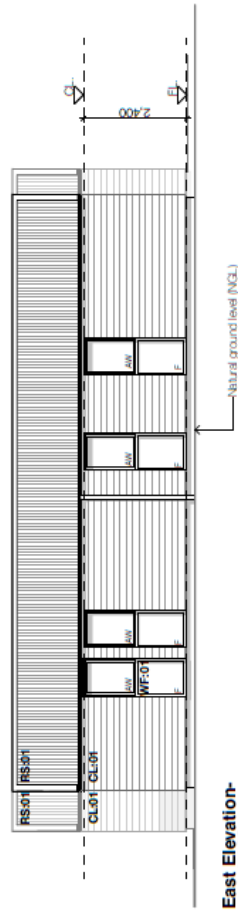
SCHEDULE

- CL-01 Fibre Cement
Type: JH Linea
Colour: Sunmist
Finish: Paint
- RS-01 Roofing
Deck: Corrugated metal 0.42bmt
Colour: Sunmist
- WF-01 Window and External Doors
Type: Double glazed aluminium frame
Glass tint: TBC
Frame Colour: White

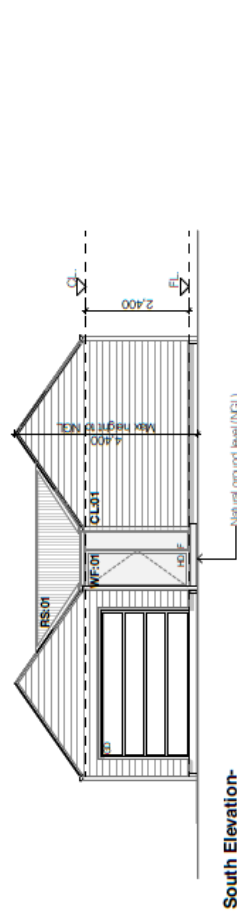
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Scale: 1:100



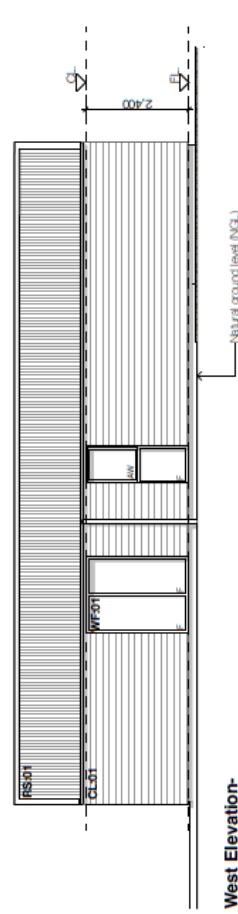
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East Elevation-
Scale: 1:100



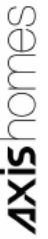
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West Elevation-
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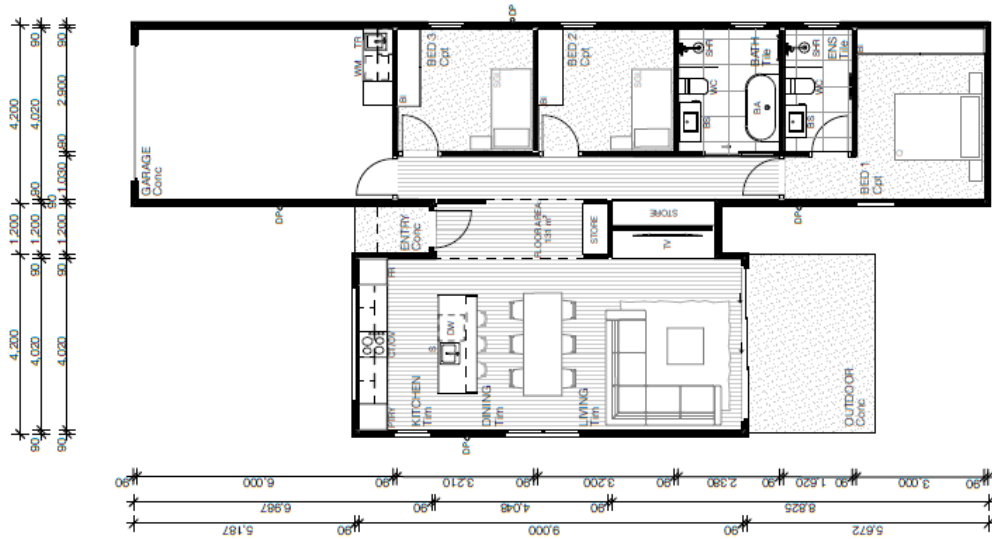
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PAGE
04 of 10



Floor Plan- Proposed Dwelling- Type B
Scale: 1:100



LEGEND

- Proposed wall
- - Existing levels
- New levels
- FL - Finished floor level (top of particle board / slab)

- Fixtures & Fittings**
- AC Air Conditioner
 - AC-OUT Air Conditioner- Outdoor Unit
 - BI Built-in Cupboard
 - BS Basin
 - CJ Control joint
 - COB Carpets
 - COL Colours
 - CT Cooktop
 - CL Ceiling level
 - CP Ceiling
 - DP Down pipe
 - FLL Finished Floor level
 - FR Fridge
 - FP Fire Pit
 - GB Glass balustrade
 - GD Garage Door
 - GRD Grated Drain
 - HWS Hot Water System
 - LDRY Laundry
 - LS Light Switch
 - NSN NBN Modem
 - NSN-EXT NBN connection box
 - OV Oven
 - RWH Rainwater Head
 - SHR Shower
 - SK Skylight
 - S Sink
 - TAP Water Tap
 - TR Trough
 - TV Television
 - WC Toilet
 - WH Heater
 - WM Washing Machine
 - WR Walk-in Robe
 - WP Walk-in Pantry

- Materials/ Finish**
- Conc Concrete paving slab.
 - Chg Chipping
 - Flt Flanking
 - Scrn Screen
 - Tile Floor tile
 - Tim Flank flooring

GENERAL NOTES

Finished landscaping must prevent water build up near footings, onto nearby structures and the ground surface. All drainage systems must be installed in accordance with Australian Standards.

All works must be in accordance with relevant Australian Standards. The Builder/Builder's representative must ensure that all works comply with the relevant regulatory bodies including local council.

Refer to Builder's scope of works for joinery inclusions.



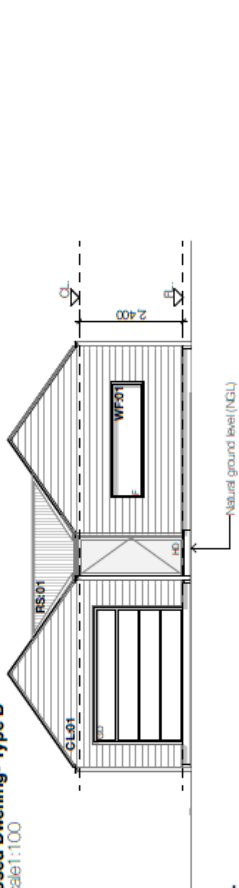
LEGEND

- AW - Awning window
- BB - Batten balustrade
- CJ - Control joint
- CL - Cladding
- DP - Downpipe
- F - Fixed window
- GB - Glass balustrade
- GD - Garage door
- FD - Glass door
- FL - Floor level
- LS - Landing
- SL - Sliding door
- SW - Stair window
- TAP - Water tap
- WB - Wire balustrade

SCHEDULE

- CL-01 Fibre Cement
Type: J1 Linea
Colour: Sunmist
Finish: Paint
- RS-01 Roofing
Deck: Corrugated metal 0.42bmt
Colour: Sunmist
- WF-01 Window and External Doors
Type: Double glazed aluminium frame
Glass Tint: TBC
Frame Colour: White

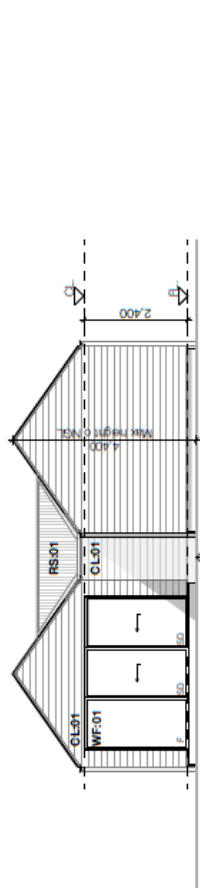
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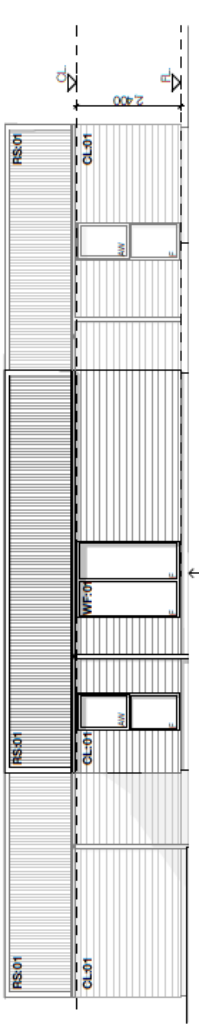
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East Elevation-
scale:1:100



South Elevation-
scale:1:100



West Elevation-
scale:1:100

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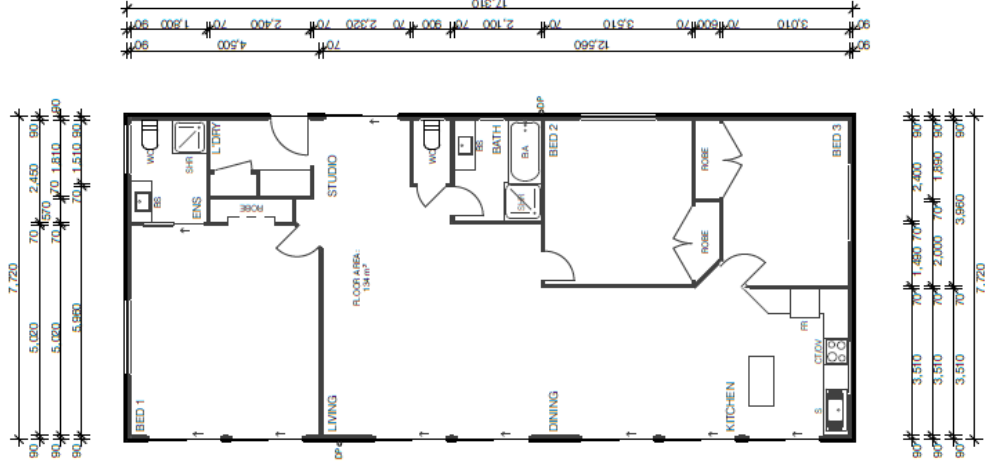
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 PAGE
 06 of 10



Floor Plan- Proposed Dwelling- Type C
Scale: 1:100



LEGEND

- Proposed wall
- ◊ Existing levels
- ◊ New levels
- FL - Finished floor level (top of particle board / slab)

Fixtures & Fittings

- AC - Air Conditioner
- AC-OUT - Air Conditioner- Outdoor Unit
- BI - Built-in Cupboard
- BS - Bash
- CJ - Control joint
- CT - Cooktop
- COL - Column
- CL - Ceiling
- CL-LEVEL - Ceiling level
- DP - Down pipe
- FLL - Finished Floor level
- FR - Fridge
- FP - Fire Pit
- GB - Glass balustrade
- GD - Garage Door
- GRD - Grated Drain
- HWS - Hot Water System
- LAB - Laundry
- MA - Manifold
- NSN - NBN Modem
- NSN-EXT - NBN connection box
- OV - Oven
- RWH - Rainwater Head
- SHR - Shower
- SK - Skylight
- S - Sink
- WATER TAP - Water Tap
- TR - Trough
- TV - Television
- WC - Toilet
- WH - Wood Heater
- WM - Washing Machine
- WR - Walk-in Robe
- WIP - Walk-in Pantry

Materials/ Finishes

- Carpet - Carpet paving slab.
- Carpet - Carpet
- Paint - Paint
- Screen - Screen
- Stair - Stair
- Tim - Timber
- Plank flooring - Plank flooring

GENERAL NOTES

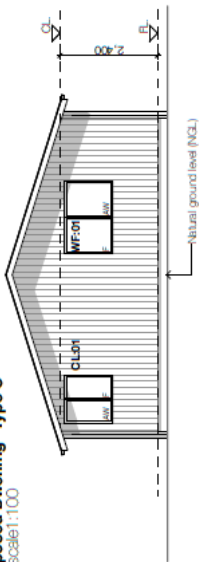
1. All works must be in accordance with relevant Australian Standards, the National Building Code, Australian Standards, and any other relevant regulatory bodies including local council.
2. Refer to Builder's scope of works for pinery inclusions.



- LEGEND**
- AW - Awning window
 - BB - Ballen balustrade
 - CB - Ceiling
 - CL - Ceiling Panel
 - DP - Downpipe
 - EW - Eaved window
 - GD - Garage door
 - GL - Glass door
 - GS - Sliding glass door level
 - SD - Sliding door
 - SW - Sliding window
 - WP - Window Panel
 - WB - Wire balustrade

- SCHEDULE**
- CL-01 Fibre Cement
Type: JH Sycron Axon
Colour: Sunflist
Finish: Paint
 - RS-01 Roofing
Colour: Sunflist
Colour: Sunflist
 - WF-01 Window and External Doors
Type: 40 Series
Glass Tint: TC
Frame Colour: White

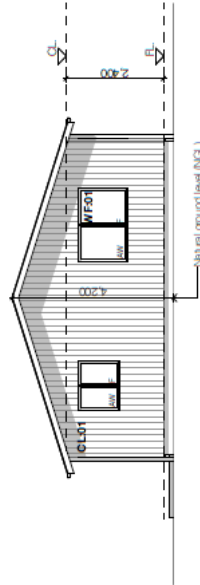
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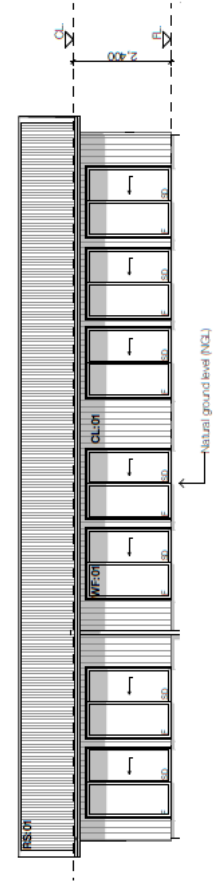
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East Elevation-
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South Elevation-
scale:1:100



West Elevation-
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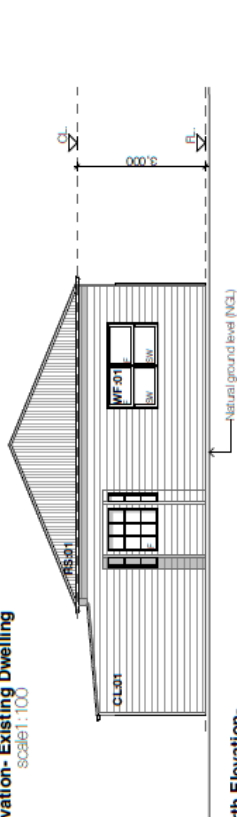
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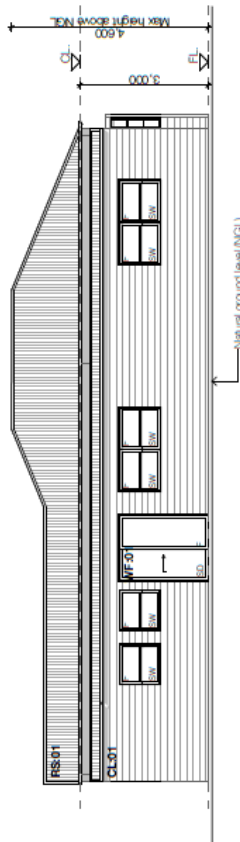


- LEGEND**
- RV - Railing window
 - WB - Wire balustrade
 - BB - Glass balustrade
 - CL - Ceiling level
 - CP - Power cable
 - FW - Filled window
 - GB - Glass balustrade
 - GD - Garage door
 - FL - Finished floor level
 - FD - Finished floor level
 - SD - Sliding door
 - SW - Sliding window
 - WB - Wire balustrade
- SCHEDULE**
- CL-01 Weatherboard
Colour: White
Finish: Paint
 - RS-01 Roofing
Decks: Corrugated metal 0.42/tonit
Colour: Surfmat
 - WF-01 Window and External Doors
Type: Double glazed aluminium frame
Glass Tint: TBC
Frame Colour: White

Elevation- Existing Dwelling
scale:1:100



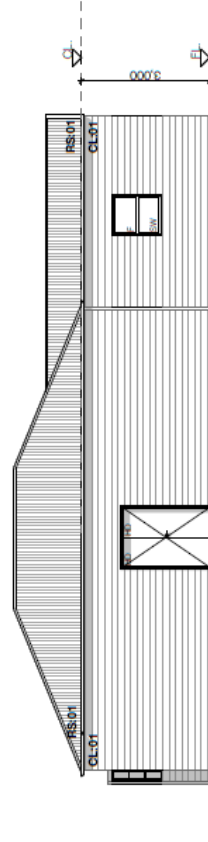
North Elevation-
scale:1:100



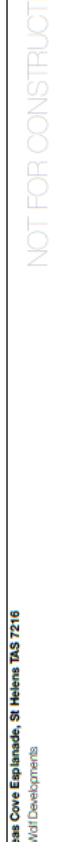
East Elevation-
scale:1:100



South Elevation-
scale:1:100



West Elevation-
scale:1:100



32 Medeas Cove Esplanade, St Helens TAS 7216
Thomas Wolf Developments

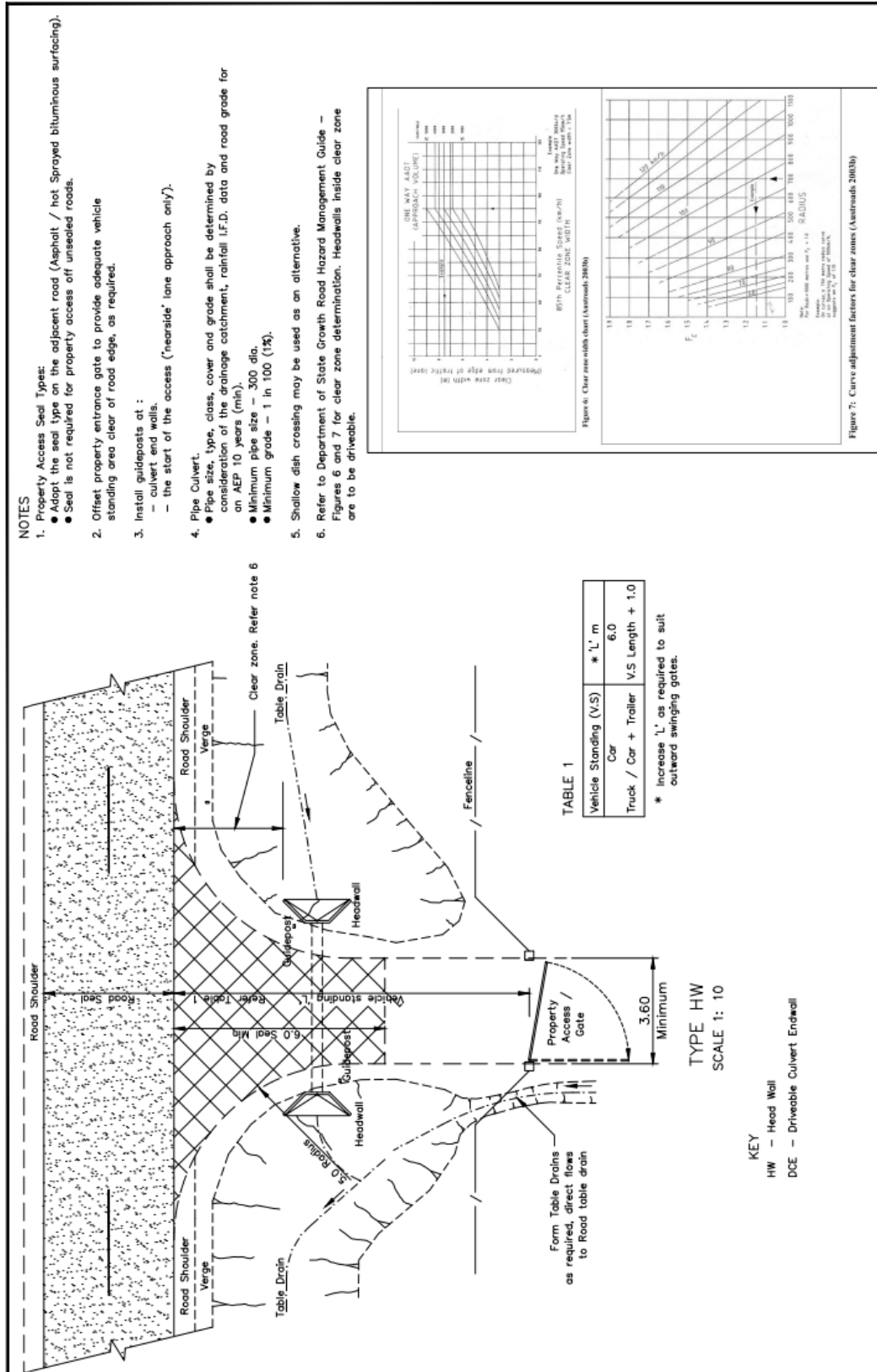
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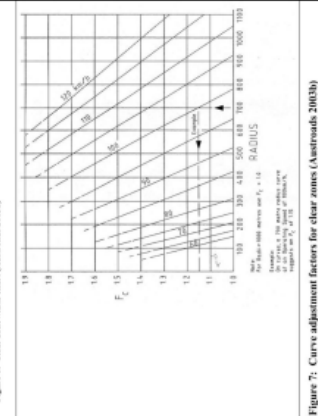
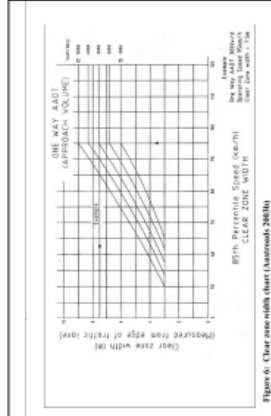
Date Published: 24/02/2026
Drawing Status: DA
Drawing Scale: 1:100
Drawn by:
Revision
PAGE
10 of 10

Appendix E – Rural Road Property access



NOTES

- Property Access Seal Types:
 - Adopt the seal type on the adjacent road (Asphalt / hot Sprayed bituminous surfacing).
 - Seal is not required for property access off unsealed roads.
- Offset property entrance gate to provide adequate vehicle standing area clear of road edge, as required.
- Install guideposts at :
 - culvert end walls.
 - the start of the access ('nearside' lane approach only).
- Pipe Culvert.
 - Pipe size, type, class, cover and grade shall be determined by consideration of the drainage catchment, rainfall I.F.D. data and road grade for an AEP 10 years (min).
 - Minimum pipe size – 300 dia.
 - Minimum grade – 1 in 100 (1%).
- Shallow dish crossing may be used as an alternative.
- Refer to Department of State Growth Road Hazard Management Guide – Figures 6 and 7 for clear zone determination. Headwalls inside clear zone are to be drivable.



STANDARD DRAWING
RURAL ROADS
TYPICAL PROPERTY ACCESS

XRef File: TSD-R03-v3.dwg
 Scales: AS SHOWN (All scales are correct at A3)
 REFERENCES
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TSD-R03-v3
 18-09-2020

