

Development Applications

Notice is hereby given under Section 57(3) of the *Land Use Planning & Approvals Act 1993* that an application has been made to the Break O' Day Council for a permit for the use or development of land as follows:

DA Number	DA 2026 / 00049
Applicant	A McArthur
Proposal	Residential - Construction of a New Dwelling, Second Vehicle Access, Associated Driveway and Water Tank.
Location	14 Hilltop Drive, Binalong Bay (CT 80821/19)

Plans and documents can be inspected at the Council Office by appointment, 32 – 34 Georges Bay Esplanade, St Helens during normal office hours or online at www.bodc.tas.gov.au.

Representations must be submitted in writing to the General Manager, Break O' Day Council, 32 -34 Georges Bay Esplanade, St Helens 7216 or emailed to admin@bodc.tas.gov.au, and referenced with the Application Number in accordance with section 57(5) of the abovementioned Act during the fourteen (14) day advertised period commencing on Saturday 4th April 2026 **until 5pm Tuesday 21st April 2026**.

John Brown
GENERAL MANAGER

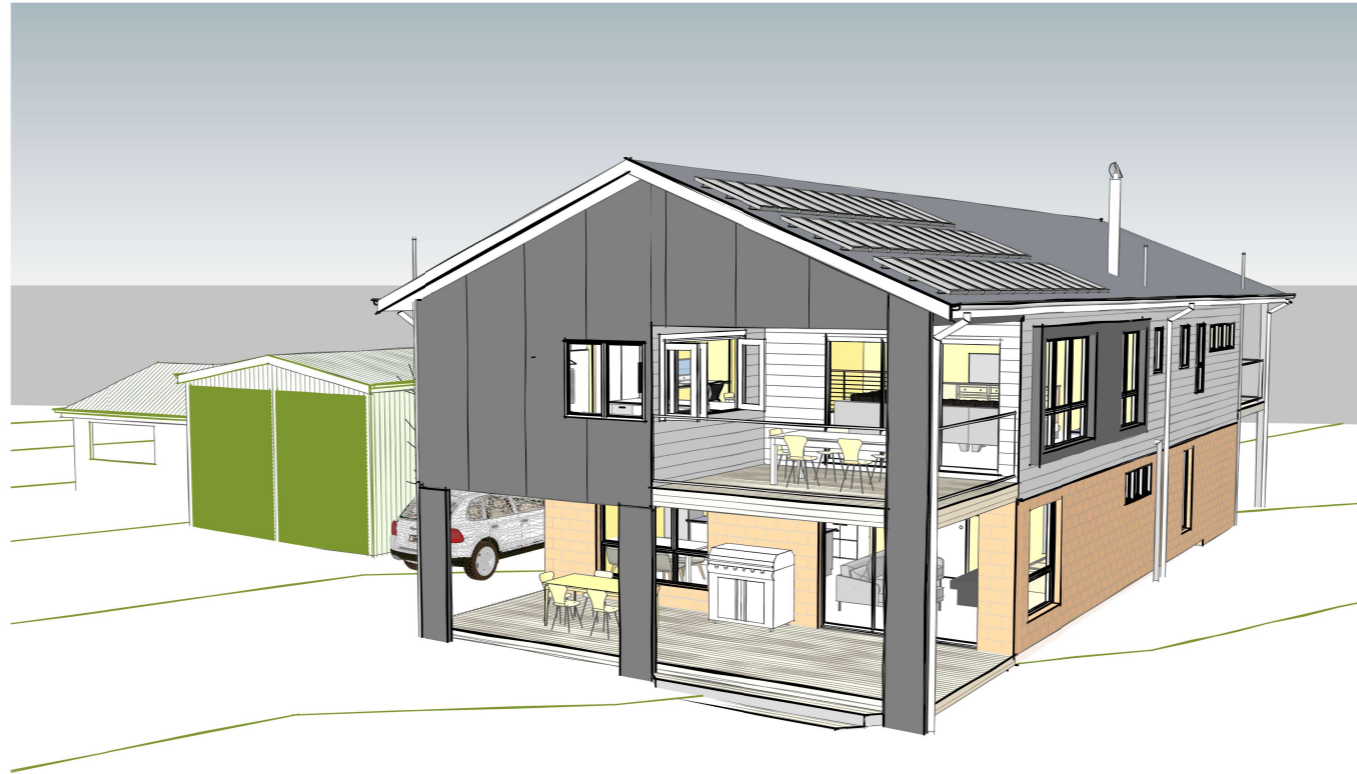
PROPOSED NEW DWELLING

at

14 HILLTOP DRIVE, BINALONG BAY

for

MR J. LAMBERT & MRS P. LOONE



PROJECT DETAILS

COUNCIL:- BREAK O DAY COUNCIL
 ZONE:- 10.0 LOW DENSITY RESIDENTIAL
 PLANNING OVERLAY:-
 -STORMWATER MANAGEMENT SPECIFIC AREA PLAN (2)
 -NATURAL ASSETS CODE (7) - PRIORITY VEGETATION AREA
 PID:- 3143330
 TITLE REF:- 80821/19
 CLIMATE ZONE:- 7
 WIND SPEED:- N3 - REFER GES SITE CLASSIFICATION REPORT
 SITE CLASSIFICATION:- 'M' - REFER GES SITE CLASSIFICATION REPORT
 BAL RATING:- N/A
 CORROSIVE ENVIRONMENT:- 230m FROM BREAKING SURF

SITE AREAS

SITE:- 2025.733m sq.
 EXISTING SHED:- 72.5m sq.
 EXISTING GARDEN SHED:- 28.5m sq.
 EXISTING BBQ SHELTER:- 24m sq.
 PROPOSED NEW DWELLING:- 242.1m sq.
 TOTAL DEVELOPED AREA:- 367.1m sq.
 SITE COVERAGE:- 18.12%

DRAWING SCHEDULE

- 1:- COVER PAGE
- 2:- SITE LISTMAP IMAGE
- 3:- OVERALL LOCATION PLAN
- 4:- SITE PLAN
- 5:- DRAINAGE PLAN - LOWER FLOOR
- 6:- DRAINAGE PLAN - UPPER FLOOR
- 7:- LOWER FLOOR PLAN
- 8:- UPPER FLOOR PLAN
- 9:- 3D IMAGES
- 10:- 3D IMAGES
- 11:- LOWER 3D FLOOR PLAN
- 12:- UPPER 3D FLOOR PLAN
- 13:- ELEVATIONS
- 14:- ELEVATIONS
- 15:- ROOF PLAN

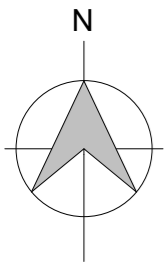
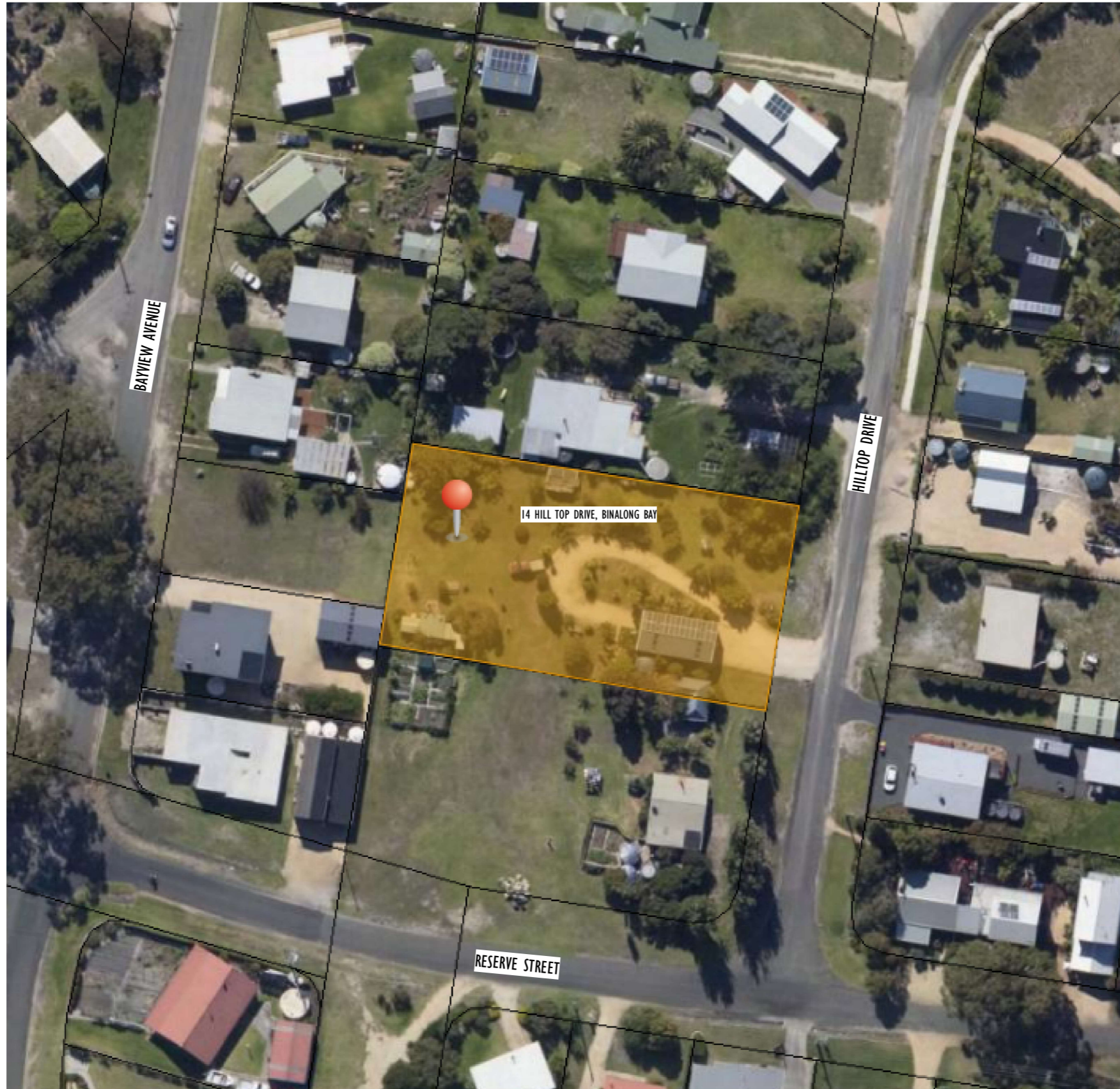
No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

PAM LOONE & JOHN LAMBERT
 14 HILLTOP DRIVE, BINALONG BAY

Cover Page

Project number	A26001	1
Date	23.3.26	
Drawn by	AMc	
Checked by	CT	

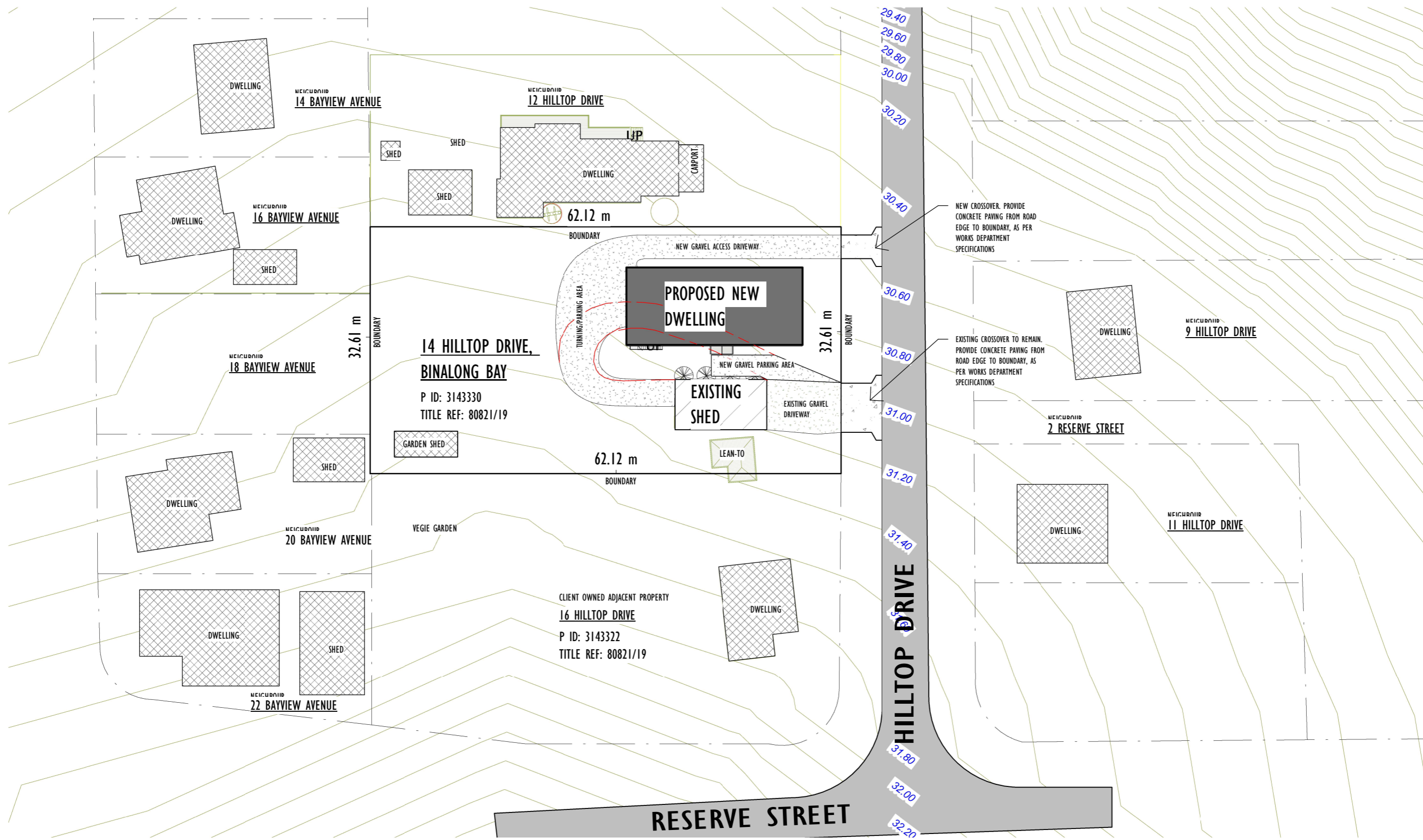
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DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

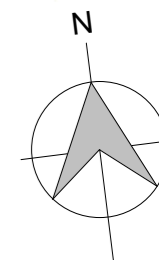
PAM LOONE & JOHN LAMBERT
 14 HILLTOP DRIVE, BINALONG BAY

SITE LISTMAP IMAGE		
Project number	A26001	2
Date	23.3.26	
Drawn by	AMc	
Checked by	CT	
Scale		



OVERALL LOCATION PLAN

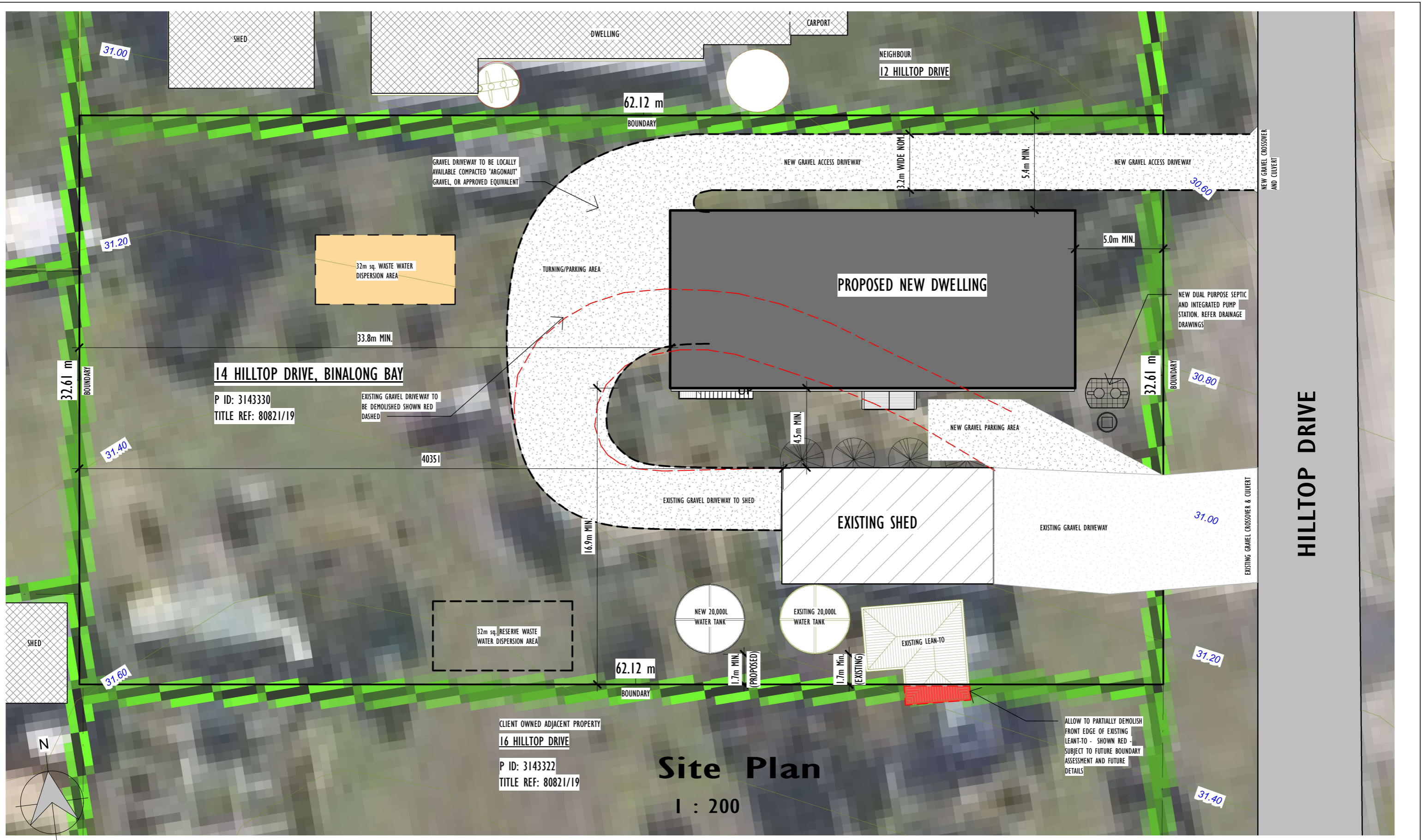
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No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

PAM LOONE & JOHN LAMBERT
 14 HILLTOP DRIVE, BINALONG BAY

OVERALL LOCATION PLAN		3
Project number	A26001	
Date	23.3.26	
Drawn by	APM	
Checked by	CT	
Scale		1 : 500



Site Plan

1 : 200

No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

PAM LOONE & JOHN LAMBERT
14 HILLTOP DRIVE, BINALONG BAY

SITE PLAN		4
Project number	A26001	
Date	23.3.26	
Drawn by	AM	
Checked by	CT	
Scale	1 : 200	

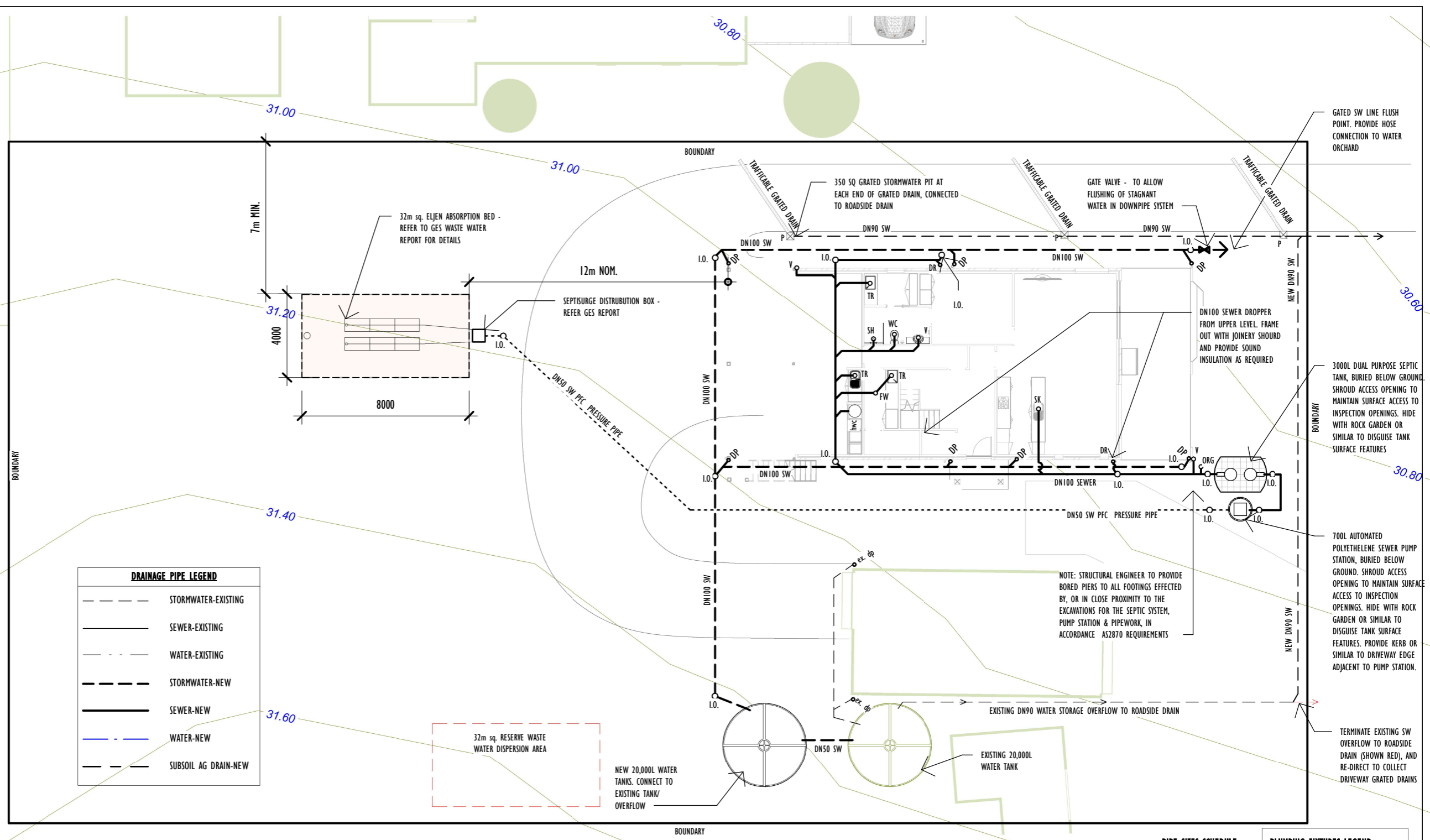
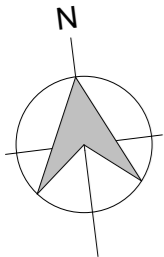
PLUMBING AND DRAINAGE NOTES

- ALL PLUMBING WORK TO BE CARRIED OUT BY A LICENCED PLUMBER, AND COMPLY WITH AS 3500, THE TASMANIAN PLUMBING CODE, THE TASMANIAN PLUMBING REGULATIONS, AND BE IN ACCORDANCE WITH THE LOCAL AUTHORITIES STANDARD PRACTICE AND APPROVAL.
- THE PLUMBER/BUILDER IS TO CONFIRM ON SITE THAT DRAINAGE LINES CAN ACHIEVE REQUIRED FALLS TO LEGAL POINTS OF DISCHARGE, WITH APPROPRIATE COVER.
- HOT WATER FROM THE HW CYLINDER IS TO BE TEMPERED TO 50°C FOR ALL FIXTURES EXCEPT KITCHEN AND LAUNDRY, WHICH IS 60°C.
- HOT & COLD RETICULATION LINES TO BE DN20 WITH DN15 BRANCHES TO INDIVIDUAL FIXTURES. PROVIDE DN25 LINE FROM METER TO HOUSE, IF APPLICABLE.
- CREATE FALLS ON SITE DRAIN ALL SURFACE WATER AWAY FROM FOOTINGS IN ACCORDANCE WITH AS 2870. BUILDER TO PROVIDE SITE FALLS AS REQUIRED TO DO SO.
- THE BUILDING CONTRACTOR MUST LOCATE THE EXISTING CONNECTION POINTS TO THE MAINS TO VERIFY THAT THEIR POSITIONS & DEPTHS ARE CONSISTENT WITH DIAL BEFORE YOU DIG INFORMATION. SUCH VERIFICATION MUST BE COMPLETED BEFORE COMMENCING CONSTRUCTION.
- INSTALLATION OF ORG IS TO COMPLY WITH AS3500 PART 2 CLAUSES 4.6.6.6 (MINIMUM HEIGHT BELOW LOWEST FIXTURE = 150MM) & 4.6.6.7 (MINIMUM HEIGHT ABOVE SURROUNDING GROUND FINISHED SURFACE LEVEL = 75MM).
- NEW SEWER = DN100 PVC @ 1:60 FALLS MIN.
- NEW STORMWATER = DN100 PVC @ 1:100 FALLS MIN.(UNLESS NOTED OTHERWISE)
- PAVED AND HARDSTAND AREAS TO BE DRAINED TOWARDS SURFACE PITS, AND CONNECTED TO THE MAINS SYSTEM.
- STANDARD DRAIN SIZES
SINK: DN50
WC: DN100
STORMWATER: DN100
- WATER PIPE SIZES
COLD WATER: DN 20 WITH DN16 BRANCHES
HOT WATER: DN 20 WITH DN 16 BRANCHES
- ALL DRAINAGE WORKS REQUIRE TSWATER AND COUNCIL APPROVAL, AND TO BE IN GENERALY IN ACCORDANCE WITH THE WATER SUPPLY CODE OF AUSTRALIA.

PLUMBING NOTES

REACTIVE SITES - WHERE SERVICE PIPES PENETRATE THROUGH EXTERNAL FOOTINGS, PIPES ARE TO BE LAGGED & INSTALLED THROUGH THE MIDDLE THIRD PORTION OF THE FOOTINGS ONLY. FLEXIBLE CONNECTIONS ARE TO BE PROVIDED ADJACENT TO THE FOOTINGS PRIOR TO CONNECTION TO THE DRAINAGE TO COMPLY WITH AS2870-2010 SECTION 5.6. ADDITIONAL REQUIREMENTS FOR CLASS M,H & E SITES. REFER ENGINEER FOR DETAILS.

DRAINAGE PIPE LEGEND	
	STORMWATER-EXISTING
	SEWER-EXISTING
	WATER-EXISTING
	STORMWATER-NEW
	SEWER-NEW
	WATER-NEW
	SUBSOIL AG DRAIN-NEW



LOWER FLOOR DRAINAGE PLAN

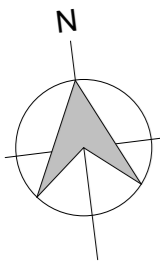
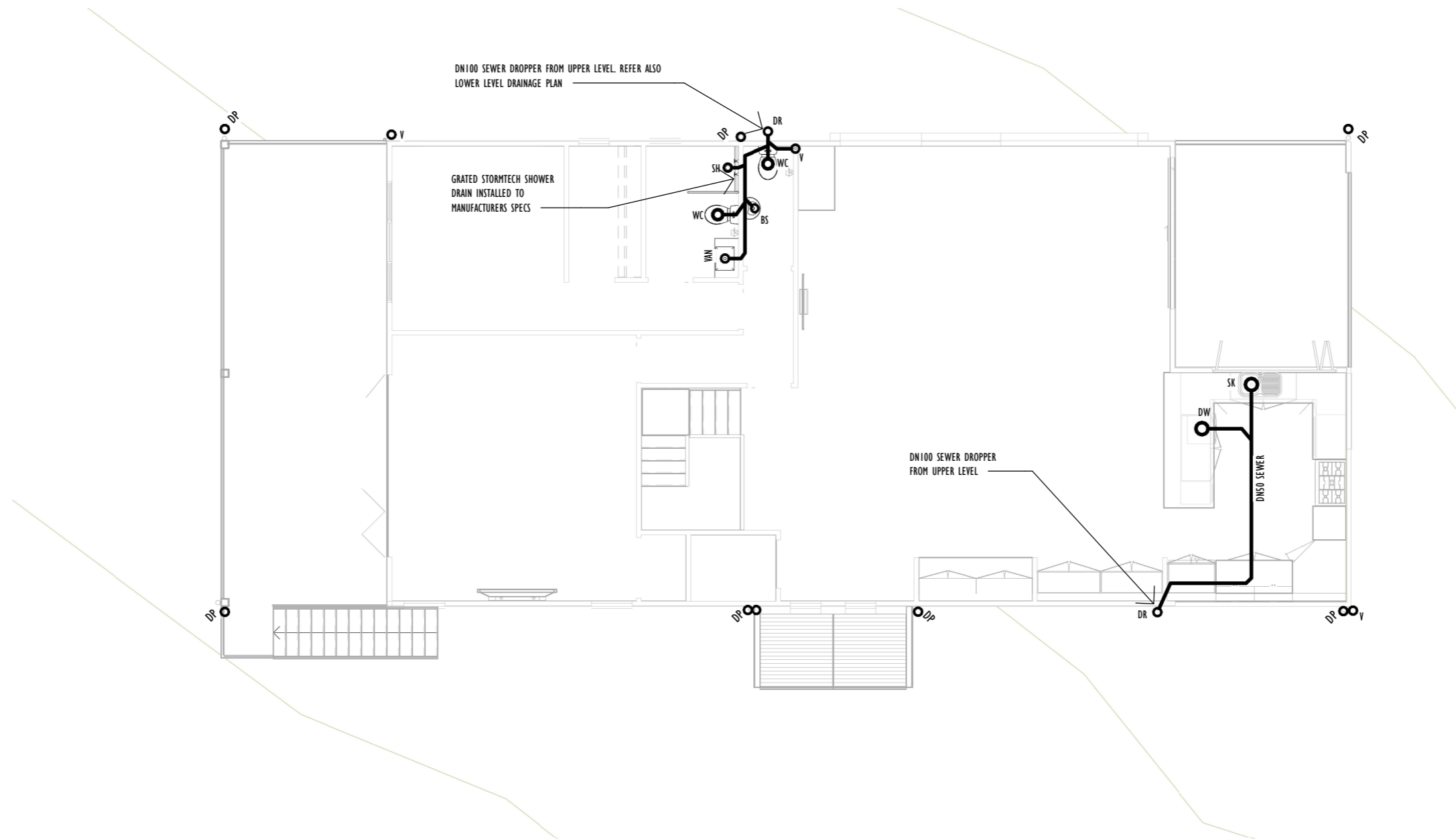
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PIPE SIZES SCHEDULE		PLUMBING FIXTURES LEGEND	
SW - DN100 PVC		a. -WC	
SEWER - DN100 PVC		b. -BASIN / VANITY	
TROUGH - DN50		c. -SHOWER	
SINK - DN50		dr. -DN100 DROPPER FROM FLOOR ABOVE	
WC - DN100		hwc. -HOT WATER CYLINDER OVERFLOW	
		fw. -FLOOR WASTE	
		tr. -TROUGH	
		i.o. -INSPECTION OUTLET	
		org - OVERFLOW RELEIF GULLY	
		dp - DOWNPIPE	
		v -DN50 VENT TO AIR	

No.	Description	Date
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DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

PAM LOONE & JOHN LAMBERT 14 HILLTOP DRIVE, BINALONG BAY		
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DRAINAGE PLAN - L.F.		5
Project number	A26001	
Date	23.3.26	
Drawn by	AM	
Checked by	CT	
Scale	1 : 200	



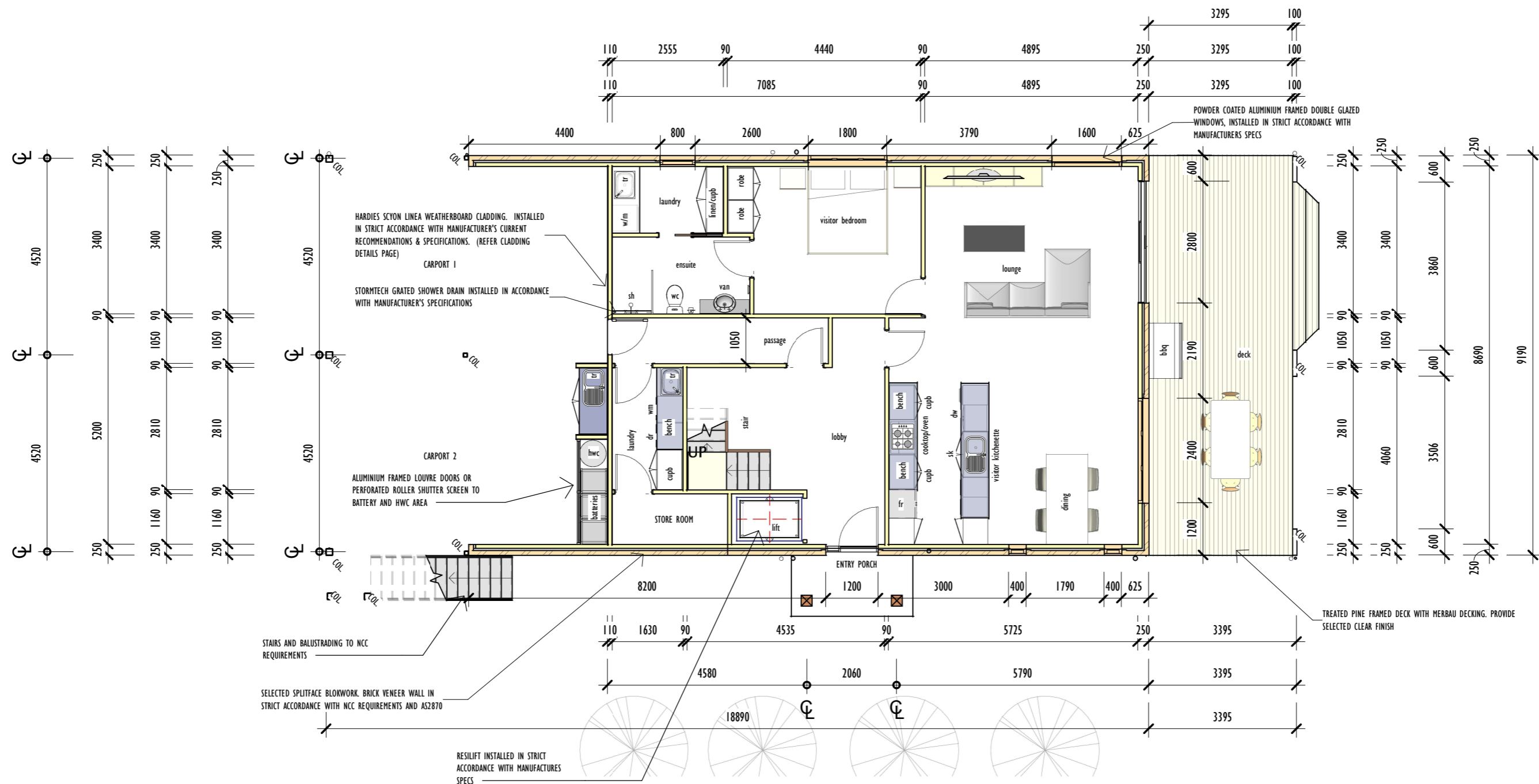
UPPER FLOOR DRAINAGE PLAN

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No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

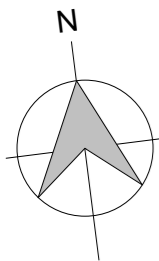
PAM LOONE & JOHN LAMBERT
14 HILLTOP DRIVE, BINALONG BAY

DRAINAGE PLAN - U.F.		6
Project number	A26001	
Date	23.3.26	
Drawn by	AM	
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LOWER FLOOR PLAN

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No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

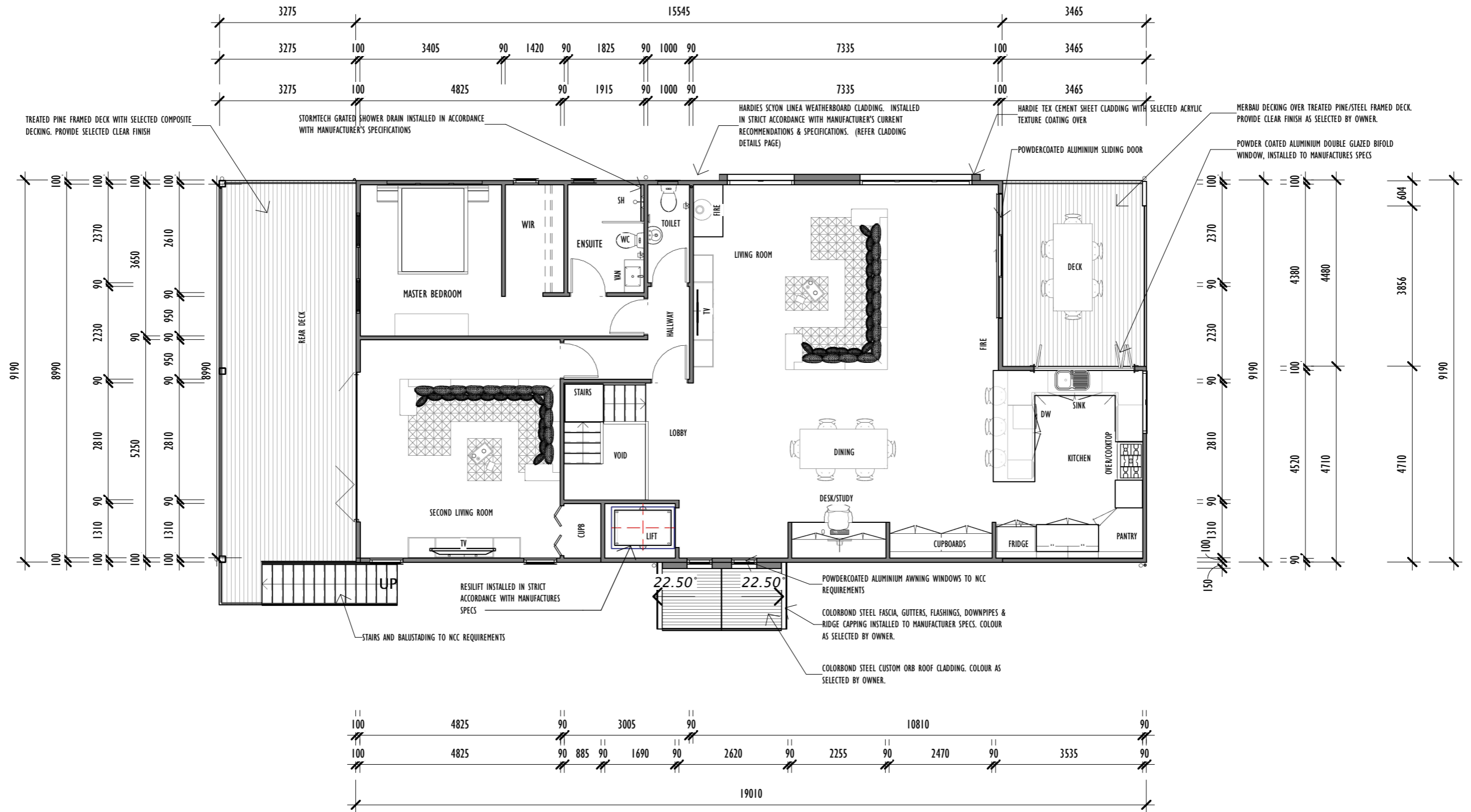
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14 HILLTOP DRIVE, BINALONG BAY

LOWER FLOOR PLAN

Project number	A26001
Date	23.3.26
Drawn by	AM
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Scale 1 : 100



UPPER FLOOR PLAN

1 : 100

No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

PAM LOONE & JOHN LAMBERT
14 HILLTOP DRIVE, BINALONG BAY

UPPER FLOOR PLAN

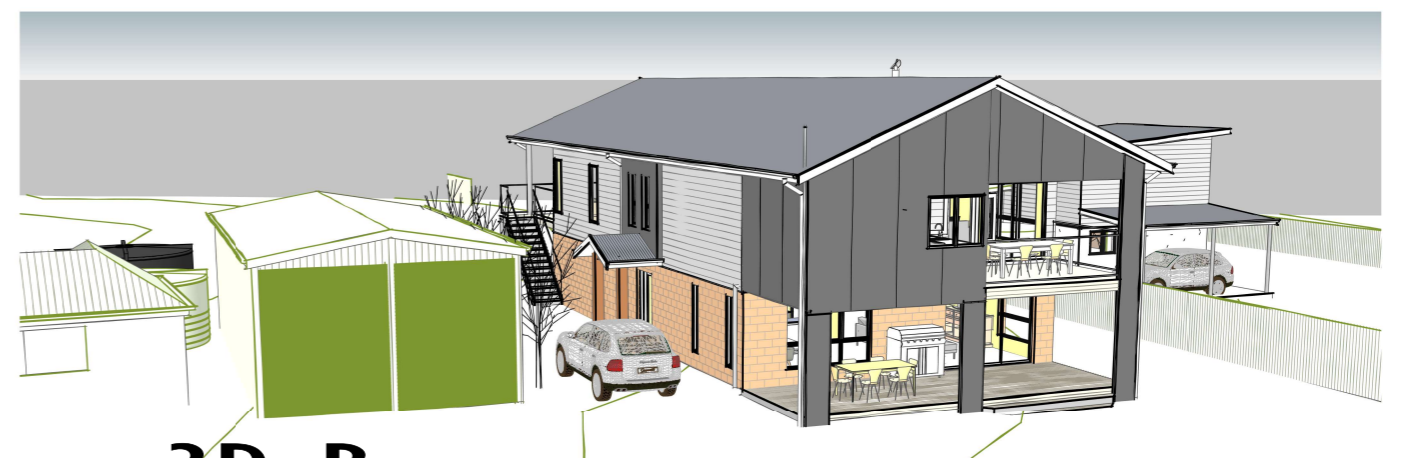
Project number	A26001
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Drawn by	AM
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3D A



3D B



3D C



3D D

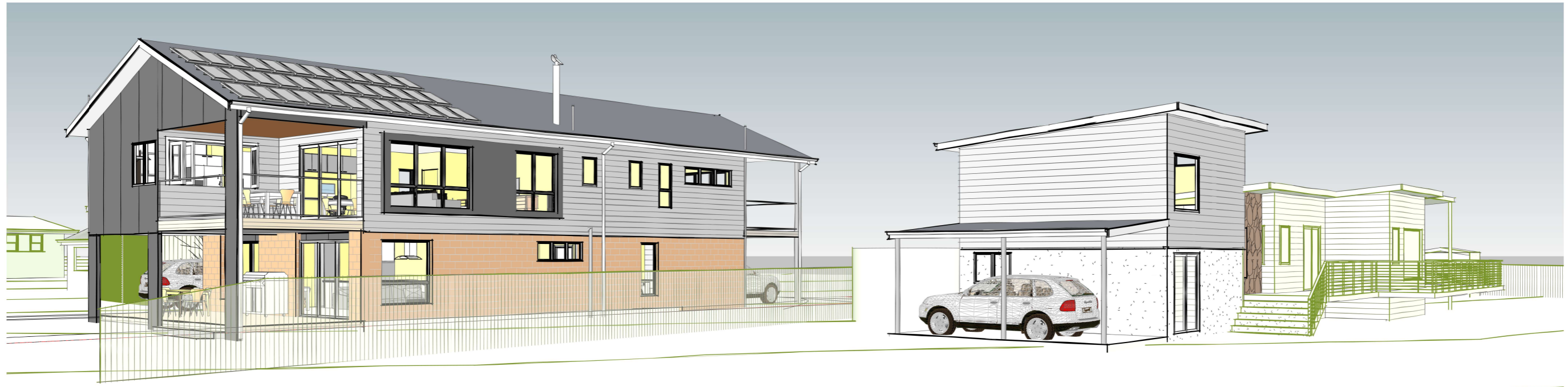
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PAM LOONE & JOHN LAMBERT
14 HILLTOP DRIVE, BINALONG BAY

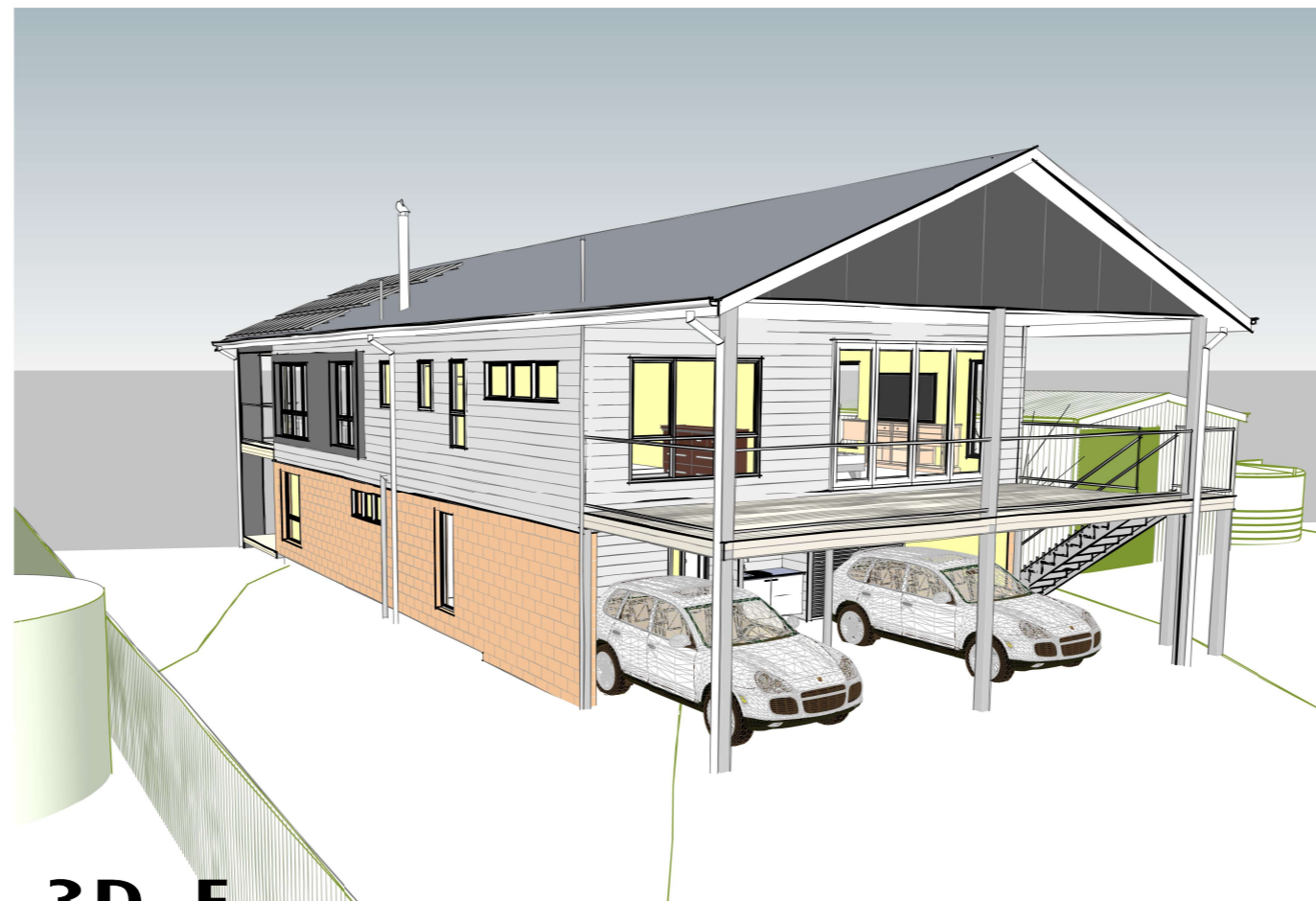
3D IMAGES

Project number	A26001
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Drawn by	AM
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Scale



3D E



3D F

No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

PAM LOONE & JOHN LAMBERT
14 HILLTOP DRIVE, BINALONG BAY

3D IMAGES

Project number	A26001
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Drawn by	AM
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Scale



LOWER FLOOR 3D FLOOR PLAN

No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

PAM LOONE & JOHN LAMBERT
14 HILLTOP DRIVE, BINALONG BAY

LOWER FLOOR 3D PLAN

Project number	A26001
Date	23.3.26
Drawn by	AM
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Scale



UPPER FLOOR 3D FLOOR PLAN

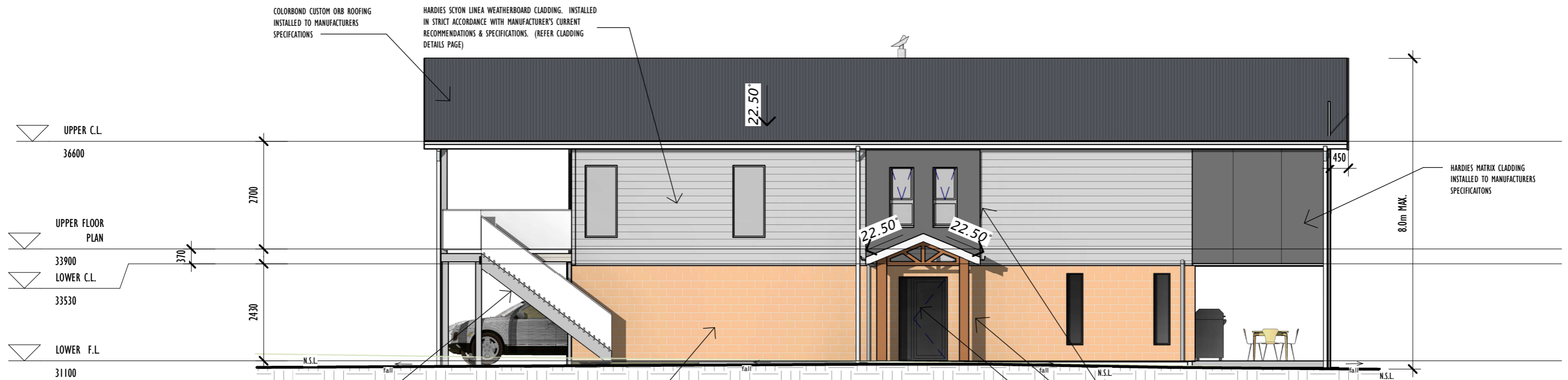
No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

PAM LOONE & JOHN LAMBERT
14 HILLTOP DRIVE, BINALONG BAY

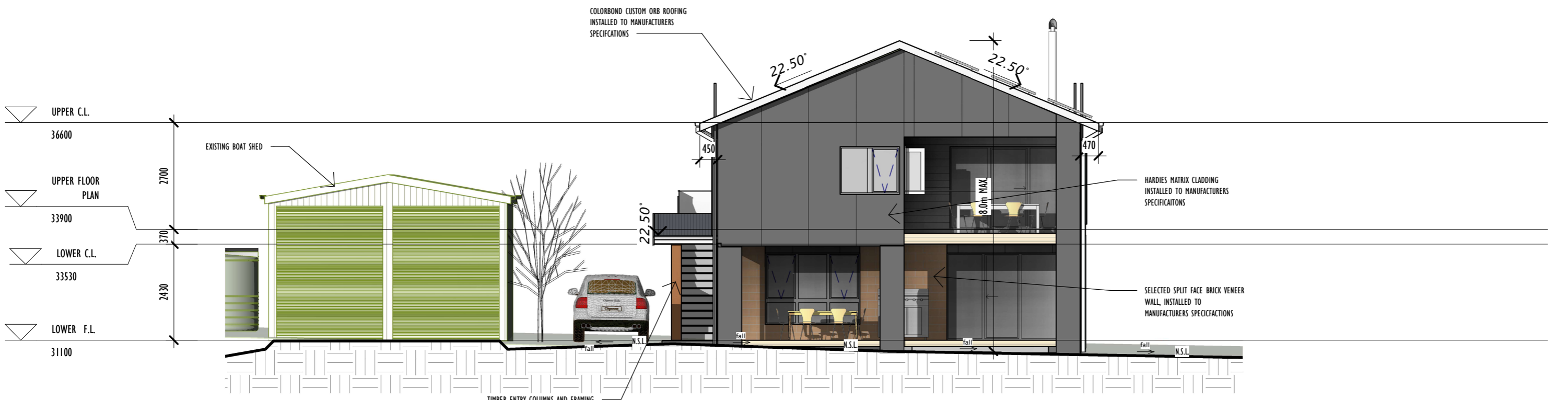
UPPER FLOOR 3D PLAN

Project number	A26001
Date	23.3.26
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12
Scale



South Elevation
1 : 100

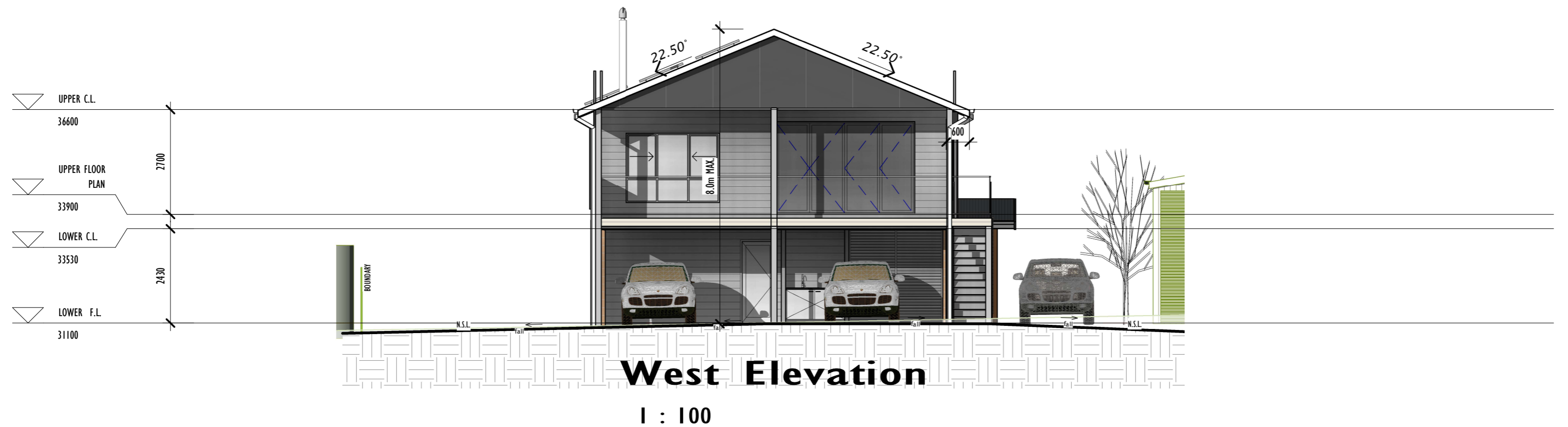
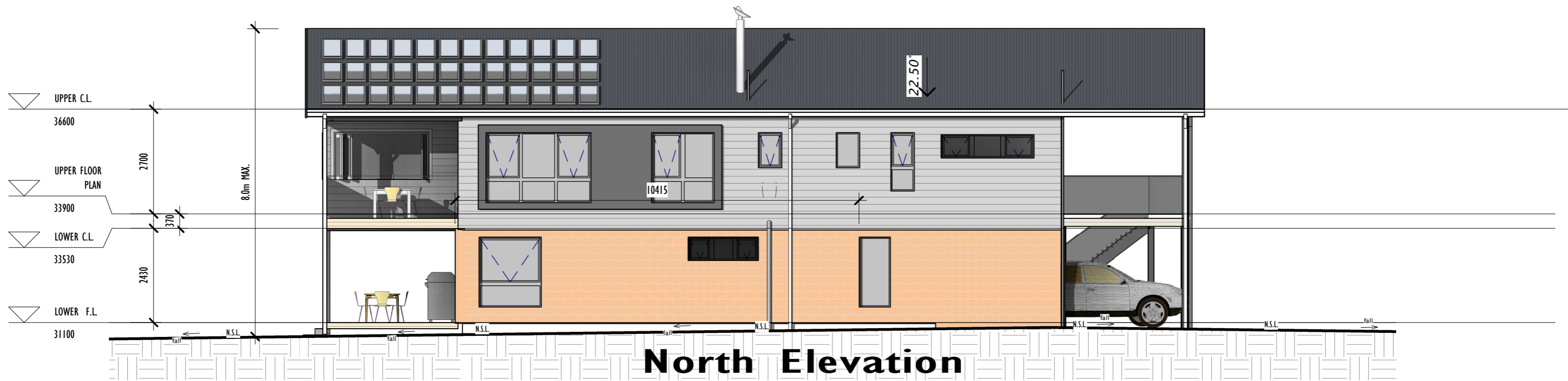


East Elevation
1 : 100

No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DAZ	REVISED FOR DA/PLANNING APPROVAL	23/3/26

PAM LOONE & JOHN LAMBERT
14 HILLTOP DRIVE, BINALONG BAY

Elevations		13
Project number	A26001	
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Drawn by	AM	
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Scale		1 : 100



No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
DA2	REVISED FOR DA/PLANNING APPROVAL	23/3/26

PAM LOONE & JOHN LAMBERT
 14 HILLTOP DRIVE, BINALONG BAY

Elevations		14
Project number	A26001	
Date	23.3.26	
Drawn by	AM	
Checked by	CT	Scale 1 : 100

ROOFING NOTES

-ROOF CLADDING IS TO COMPLY WITH THE ACCEPTABLE CONSTRUCTION METHODS OF PART 3.5 OF THE NCC.

-SHEET ROOF CLADDING TO BE IN ACCORDANCE WITH THE NCC, PART 3.5.1. AND AS1562.1. REFER ALSO CLADDING MANUFACTURERS INSTALLATION SPECIFICATIONS.

-ROOF SHEETING CREST FIXED IN ACCORDANCE WITH NCC PART 3.5.1.5. REFER MANUFACTURERS SPECIFICATIONS FOR INDIVIDUAL CLADDING TYPES

-FLASHINGS IN ACCORDANCE WITH THE NCC, PART 3.5.1.7, AND ALSO CLADDING MANUFACTURERS FLASHING AND INSTALLATION SPECIFICATIONS.

-COLORBOND ROOF CLADDING ON BATTENS AT 900 CRS. MAX. GENERALLY, REFER TO CLADDING MANUFACTURERS SPAN TABLES FOR SHEET TYPE.

-ROOF FRAMING SIZES AND TIEDOWNS AS PER TRUSS MANUFACTURERS SPECIFICATIONS AND ENGINEERS DRAWINGS.

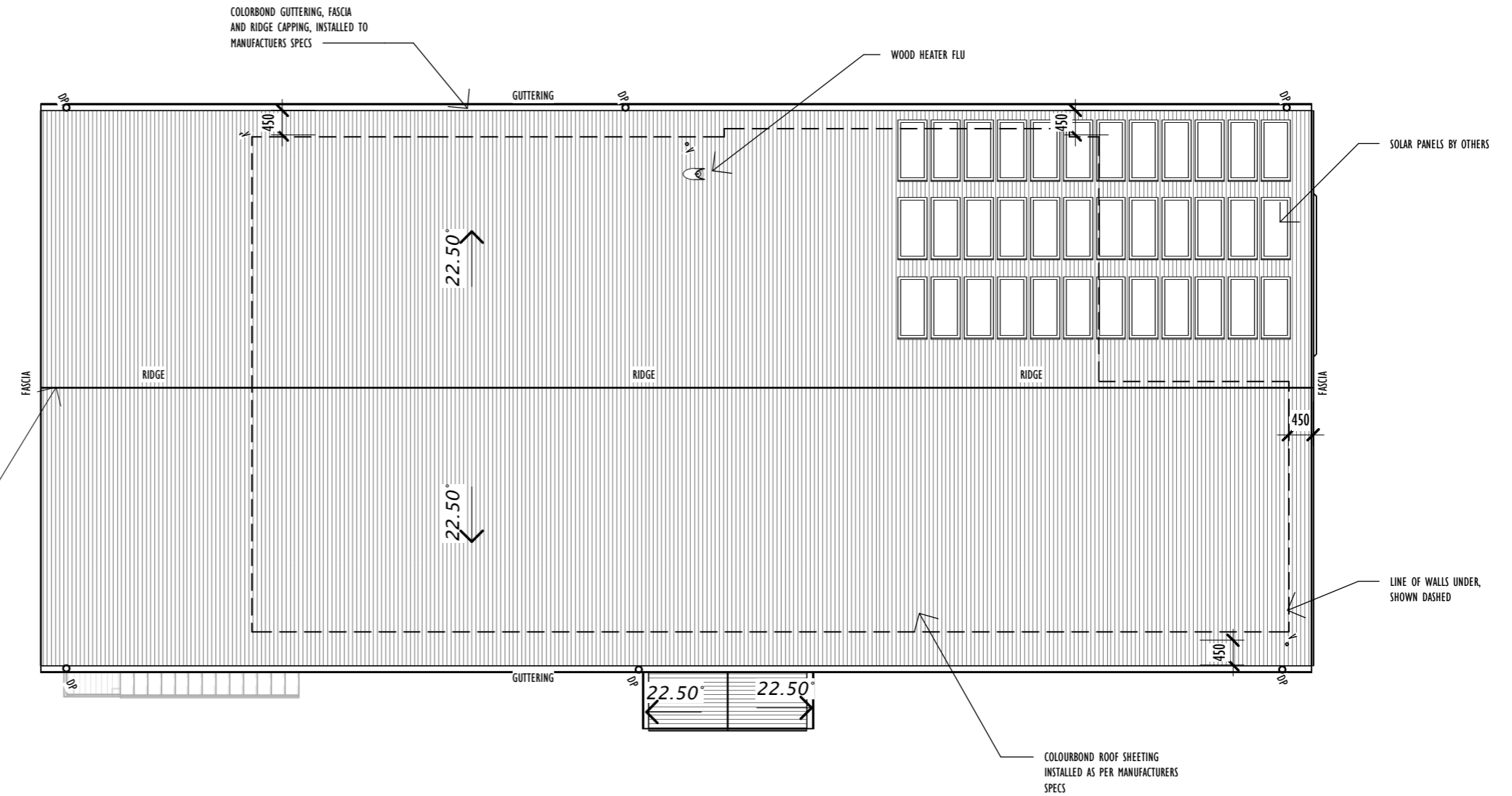
-ROOF TILES: PART 3.5.2 OF THE NCC

-GUTTERS AND DOWNPIPES DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH NCC PART 3.5.3., OR AS3500.

-COLORBOND METAL FASCIAS, GUTTERING, FLASHING AND RIDGE CAPPINGS TO BE INSTALLED IN ACCORDANCE WITH MANUFACTURERS SPECIFICATIONS, AND THE NCC PART 3.5 & AS3500 GENERALLY.

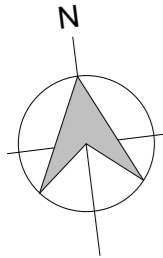
-DOWNPIPES MIN 90MM DIAM. AND TO BE INSTALLED LESS THAN 1M FROM WHERE INTERNAL OR VALLEY GUTTERS MEET THE PERIMETER GUTTER.

-GUTTERS TO BE LAPPED MIN. 75MM IN THE DIRECTION OF WATER FLOW, SEAL WITH APPROVED SEALANT AND RIVET FIXED.



Roof Plan

1 : 100



No.	Description	Date
DA	FOR DA/PLANNING APPROVAL	3/3/26
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PAM LOONE & JOHN LAMBERT
14 HILLTOP DRIVE, BINALONG BAY

Roof Plan

Project number	A26001	15
Date	23.3.26	
Drawn by	AM	
Checked by	CT	
Scale		1 : 100

Planning Compliance Report - Version 2

Date: 23/3/2026

Addressing the Tasmanian Planning Scheme

Council: Break O'Day Council

Proposed Development: New Dwelling at 14 Hilltop Drive, Binalong Bay, TAS 7216

Client: John Lambert & Pam Loone

Zone: 10.0 – Low Density Residential

Planning Overlays:- Stormwater Management Specific Area Plan (2)
Natural Assets Code (7) – Priority Vegetation Area

Introduction:

The purpose of this report is to demonstrate compliance with the relevant planning standards for a Proposed New Dwelling at 14 Hilltop Drive, Binalong Bay. This report aims to highlight how the proposal has taken into consideration the values, intent and overall objectives of the Tasmanian Planning Scheme, and address all scheme standards specifically applicable to this development. The content within is based on the development works being undertaken, completed and maintained by the applicant and owner, with the purpose of being their primary residence. The proposed development requires Performance Criteria to adequately satisfy the planning standards, and is to be read in conjunction with the supplied drawings and documentation submitted for this development.

Development Information:

This development involves the construction of a Two Bedroom Dwelling on the property at 14 Hilltop Drive, Binalong Bay.

Use Class: Residential

Site Areas:- Land Area: 2025.733m sq.
 Existing Boat Shed: 72.5m sq.
 Existing Garden Shed: 28.5m sq.
 Existing BBQ Shelter: 24m sq.
 Proposed New Dwelling: 242.1m sq.

Total Developed Area: 367.1m sq

Site Coverage: 18.12%

Planning Standards and Codes Applicable to this Development:

The following zone standards and codes of the Tasmanian Planning Scheme are applicable to this development:

-Zone 10.0 – Low Density Residential

-10.4.3 – Setback – P1 & P2

Codes:

C2.0 – Parking and sustainable transport code – C2-6.1(P1) & C2-6.3(P1)

Please Note: All other standards and codes that are not applicable to this development, or are deemed compliant with the acceptable solutions, have not been listed or detailed in this report.

State Planning Provisions

10.0 Low Density Residential – 10.4.3 – Setback

Objective: That the siting of dwellings is compatible with the streetscape and does not cause an unreasonable loss of amenity for adjoining properties.

Acceptable Solutions:

AI: - Dwellings excluding protrusions that extend not more than 0.9m into the frontage setback, must have a setback from a frontage not less than 8m.

Performance Criteria:

PI: - The siting of a dwelling must be compatible with the streetscape and character of development existing on established properties in the area, having regard to:

- (a) the topography of the site;
- (b) the setbacks of surrounding buildings;
- (c) the height bulk and form of existing and proposed buildings;
- (d) the appearance when viewed from the roads and public open space adjacent to the site; and
- (e) the safety of road users.

Response: The setback from the Hilltop Drive frontage is less than the Acceptable Solution of 8m. The building has been sited considering existing and proposed development on adjacent properties in the immediate vicinity.

- The topography of the site has a gentle fall towards the N/NE. The location of the proposed development has been sited closer to the frontage to provide a greater separation between the existing dwelling/proposed alteration on the 12 Hilltop Drive property, both to provide more privacy for both properties, and to allow for better solar access. To the immediate South of the proposed dwelling is an existing boat shed on the same title.

- The setback of this proposed dwelling is similar to those of the pre-existing dwellings on Hilltop Drive. Existing dwellings on 16 Hilltop Drive, 11 Hilltop Drive, 5 Hilltop Drive and 8 Hilltop Drive have frontage setbacks that are similar or less than the setback proposed for this development. 7 Hilltop Drive had a recent DA approval for a development with the same setback dimension as proposed here, and until recently 12 Hilltop Drive had an outbuilding with frontage setback less than what is proposed for 14 Hilltop Drive. Considering the above, the proposed setback for 14 Hilltop Drive is deemed equivalent to or greater than in some instances, to the setbacks of existing dwellings in the street.
- The height bulk and form of this proposed development will be comparable to those of the two closest neighboring properties. 12 Hilltop Drive is undergoing a two story extension to the existing dwelling, and across the road, 7 Hilltop Drive has recently completed a two storey dwelling. Cladding types and materials will be similar between all 3 adjacent properties. Given the long and narrow nature of the blocks in Hilltop Drive, it was proposed to site the development on 14 Hilltop Drive closer to the frontage to provide more separation between living areas and adjacent two storey buildings on neighboring properties.
- Due to the cladding types and materials used, the appearance will be in keeping with previously approved developments in the area.
- There will be minimal undue impact to the safety of road users caused by the siting of this development. There is no frontage boundary fence proposed for this property, and with the removal of existing garden shrubbery, site distances to and from the road will be slightly improved.

Acceptable Solutions:

A2

Dwellings, excluding outbuildings with a building height of not more than 2.4m and protrusions that extend not more than 0.9m horizontally from the building, must have a setback from side and rear boundaries of not less than 5m.

Performance Criteria:

P2

The siting of a dwelling must not cause an unreasonable loss of amenity to adjoining properties, having regard to:

- (a) The topography of the site;
- (b) the size, shape and orientation of the site;
- (c) the setbacks of surrounding buildings;
- (d) the height, bulk and form of existing and proposed buildings;
- (e) the existing buildings and private open space areas on the site;
- (f) sunlight to private open space and windows of habitable rooms on adjoining properties; and
- (g) the character of development existing on established properties in the area.

Response: While the proposed dwelling itself does have a side setback of greater than 5.0m to the side boundary, the proposed new water storage tank does not. It is proposed to sit beside the existing water tank on site. Both of which will have a setback from the southern side boundary of 1.7m min.

- The topography of the site has a gentle fall to the North/North East. The location of the proposed water tank is proposed to sit adjacent to the existing water tank already on site. This is to make use of a flatter section of the site and allow for easier connection to the existing plumbing pipework. As the existing water tank is not visible from the street, it will mean the proposed water tanks should not also be visible from the street.
- From the aerial imagery, it is clear there is a large precedence of instances where water tanks have been sited against side and rear boundary fences. This provides the best use of space on the lot, and due to their generally low height, causes little undo impact to adjoining properties in most cases. The nearest adjacent property to these new and existing water tanks is the dwelling on 16 Hilltop Drive. This dwelling has approximately 17m of separation between itself and the watertanks, and due to the tanks being screened by existing vegetation, there is minimal impact on 16 Hilltop Drive from the siting of these water tanks.
- The water tanks have a height less than 3m, they sit on the southern side of an existing boat shed with a height over 4m. The tanks are largely screened by existing vegetation, so have a diminished visual presence due to the adjacent shielding.
- Private open space for 16 Hilltop Drive surrounds the property on all sides due to its more central siting on the lot, and due to the 17m of separation between the two, there is no loss of amenity to open space areas caused by the siting of the water tanks.

- Due to the 17m separation distance from the water tanks to the habitable windows of the 16 Hilltop Drive property, and the low height and shielded nature of the water tanks location, there is no reduction of sunlight into habitable windows of 16 Hilltop Drive. With the shed and existing vegetation being substantially higher than the proposed water tank, there will be little to no loss of sunlight to open space areas on this property.
- The current water tank is not visible from the road frontage, and it is most likely that the new tank will not be visible either. There are previous instances in the local area where water tanks have been placed alongside side and front boundary fences and have become part of the streetscape. The siting of these water tanks are sited to have minimal visual impact in this regard.

C2.0 – Parking and Sustainable Transport Code

C2.6.1 - Construction of parking areas

Objective: That parking areas are constructed to an appropriate standard.

PI

All parking, access ways, manoeuvring and circulation spaces must be readily identifiable and constructed so that they are useable in all weather conditions, having regard to:

- (a) the nature of the use;
- (b) the topography of the land;
- (c) the drainage system available;
- (d) the likelihood of transporting sediment or debris from the site onto a road or public place;
- (e) the likelihood of generating dust; and
- (f) the nature of the proposed surfacing.

Response: The proposed driveway is to be constructed from compacted locally available 'argonaut' roadbase gravel. This gravel is used in multiple residential use properties all along the east coast, as well as a large proportion of high use public use spaces, such as beach car parks, boat ramp access and parking areas, camp grounds, and generally as a surface for gravel roads throughout the region. The amount of usage throughout the region proves it is fit for purpose in a range of situations and weather conditions, where in some instances pavement drainage is informal or dedicated pavement drainage is non-existent.

- The nature of use is for a residential driveway, specifically the infrequent low speed parking of vehicles, and transporting a boat to and from the property.
- The topography of the land means the driveway will have a very gentle fall to the NE/roadside, with a flatter section for parking behind the dwelling and existing shed.
- As per the site plan, grated trafficable drains are provided to the section of driveway along the northern driveway to capture stormwater from the pavement surface to prevent shedding overflow to boundary areas.
- The proposed gravel has a considerable clay element to it, so it is more stable as it becomes wet. It also compacts very well forming a hard smooth surface. The clay component means water can run off without taking a lot of fines with it. Given it is used all over the east coast in varying locations with much higher public usage, sometimes with no dedicated pavement drainage present, the suitability this product can only be deemed acceptable by its widespread and continued use throughout the region.
- Due to the low speed of vehicles travelling within the property, the likelihood of generating enough dust to cause a nuisance is low.

C2.0-6.3 – Number of Accesses for Vehicles.

Objective:

That:

- (a) access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses;
- (b) accesses do not cause an unreasonable loss of amenity of adjoining uses; and
- (c) the number of accesses minimise impacts on the streetscape.

PI

The number of accesses for each frontage must be minimised, having regard to:

- (a) any loss of on-street parking; and
- (b) pedestrian safety and amenity;
- (c) traffic safety;
- (d) residential amenity on adjoining land; and
- (e) the impact on the streetscape.

Response:

The proposed development relies on the performance criteria, as the dwelling will have two access points from Hilltop Drive. Generally, the new northern most access point will be the primary access to and from the house for residential vehicles, with parking behind the property for two cars. The existing access point in front of the existing boat shed will mainly be used when the boat is infrequently taken to and from the property, plus has a space for a visitor to park when visiting the property. The existing shed has doors in the front and back walls so the boat can be driven through the shed from behind, and negates the need to block Hilltop Drive for the purposes of reversing the boat onto the property and into the shed.

- There is no loss to parking in the area as there is no on-street parking on Hilltop Drive, and the presence of a roadside open drain prevents informal parking on the nature strip in front of this property.
- There will be no change to pedestrian safety by the additional access point as there is no footpath provided in front of this property, or directly opposite. The existing footpath doesn't start until further down the road on the opposite side of the street and will unlikely be impacted by an additional access point.
- Hilltop Drive is largely a low use residential street in a small town. Given this, and the intended use of each access point described above, traffic safety will unlikely be impacted by the additional access.
- The intended specific use of each access point means minimal impact to residential amenity of adjacent properties or to the streetscape.

